United States Army Corps of Engineers Permit

DEPARTMENT OF THE ARMY
US ARMY CORPS OF ENGINEERS
NEW ENGLAND DISTRICT
696 VIRGINIA ROAD
CONCORD MA 01742-2751

February 27, 2024

Regulatory Division
File Number: NAE-2023-00888

Jim Russell
Northern New England Passenger Rail Authority
75 West Commercial Street
Suite 104
Portland, Maine 04101
Sent by email: james@nnepra.com

Dear Mr. Russell:

The U.S. Army Corps of Engineers (USACE) has reviewed your application to place approximately 1,463 SF of permanent and 4,997 SF of temporary fill in freshwater wetlands off 696 Sanford Road at Wells, Maine in order to construct a new side platform and access to freight main line No. 2 Track. The work is shown on the attached plans entitled "USGS Site Location Map" in one sheet undated, "WELLS TRANSPORTATION CENTER" in eleven sheets dated "12/11/2023", and "Site Plans" in four sheets dated "December 7, 2023" and one sheet dated "October 13, 2022".

Based on the information that you have provided, we verify that the activity is authorized under General Permit #10, Linear Transportation Projects of the enclosed October 14, 2020, federal permits known as the Maine General Permits (GPs). The GPs are also available at https://www.nae.usace.army.mil/Missions/Regulatory/State-General-Permits/Maine-General-Permit. Please review the enclosed GPs carefully, in particular the general conditions beginning on page 5, and ensure that you and all personnel performing work authorized by the GPs are fully aware of and comply with its terms and conditions. A copy of the GPs and this verification letter shall be available at the work site as required by General Condition 33. You must perform this work in accordance with the following special condition(s):

1. You shall implement and abide by the mitigation plan, "Site Plans" in four sheets dated "December 7, 2023" and one sheet dated "October 13, 2022".

2. A status report on the implementation of the authorized work and on the construction of the mitigation shall be submitted annually to the U.S. Army Corps of Engineers, New England District, Regulatory Branch (Corps) by October 31st each year until mitigation construction is complete as determined by the Corps. This report must prominently display the reference number NAE-2023-00888.This submittal should be electronically sent to cenae-r-me@usace.army.mil.

3. An as-built mitigation construction report and as-built drawings of the mitigation area(s) shall be submitted upon completion of mitigation construction. This report must be submitted to the U.S. Army Corps of Engineers, New England District, Regulatory Branch (Corps) for review and approval and must prominently display the reference number NAE-2023-00888. The year mitigation construction is completed, as determined by the Corps, represents Year 0 for mitigation monitoring. This submittal should be electronically sent to cenae-r-me@usace.army.mil.

You must complete and return the enclosed Work Start Notification Form to this office at least two weeks before the anticipated starting date. You must complete and return the enclosed Compliance Certification Form within one month following the completion of the authorized work.

This authorization expires on October 14, 2025. You must commence or have under contract to commence the work authorized herein by October 14, 2025, and complete the work by October 14, 2026. If not, you must contact this office to determine the need for further authorization and we recommend you contact us *before* the work authorized herein expires. Please contact us immediately if you change the plans or construction methods for work within our jurisdiction as we must approve any changes before you undertake them. Performing work within our jurisdiction that is not specifically authorized by this determination or failing to comply with the special condition(s) provided above or all of the terms and conditions of the GPs may subject you to the enforcement provisions of our regulations.

This authorization does not obviate the need to obtain other federal, state, or local authorizations required by law. Applicants are responsible for applying for and obtaining any other approvals.

We continually strive to improve our customer service. To better serve you, we would appreciate your completing our Customer Service Survey located at https://regulatory.ops.usace.army.mil/customer-service-survey/

Please contact Natalie Bingham, of my staff, at our Augusta, Maine Project Office at (978) 318-8768 or natalie.bingham@usace.army.mil if you have any questions.

Sincerely,

Natalie Bingham

Digitally signed by Natalie Bingham
Date: 2024.02.27 09:49:11 -05'00'

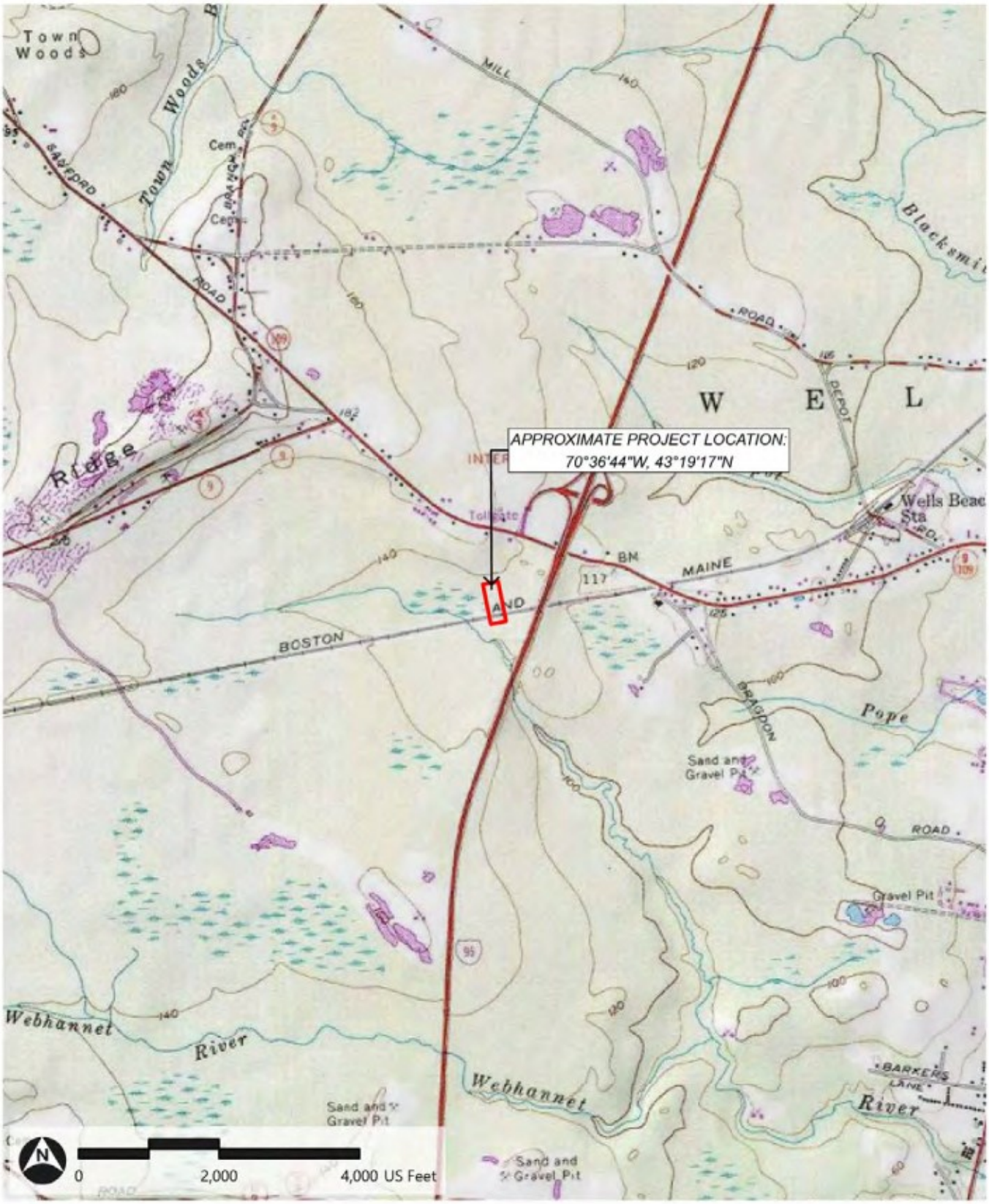
For: Peter D. Olmstead
Chief, Maine Section
Regulatory Division

Cc:

Sean Hale, VHB; shale@vhb.com

Figure 1: USGS Site Location Map

Wells Transportation Center | Wells, ME



Project Area

Source: USGS, VHB

NORTHERN NEW ENGLAND PASSENGER RAIL AUTHORITY

WELLS, MAINE

WELLS STATION EXPANSION

85% PLANS

WELLS TRANSPORTATION CENTER

INDEX OF SHEETS	SHEET
COVER SHEET	G-001
CIVIL PLANS	C-101 - C-110
ARCHITECTURAL PLANS	A-011 - AD-200
STRUCTURAL PLANS	ST-001 - ST-510
MECHANICAL PLANS	M-000 - M-600
ELECTRICAL PLANS	E-000 - E-801
PLUMBING PLANS	P-000 - P-200
LIFE SAFETY PLANS	LS-000 - LS-101

APPROVED:

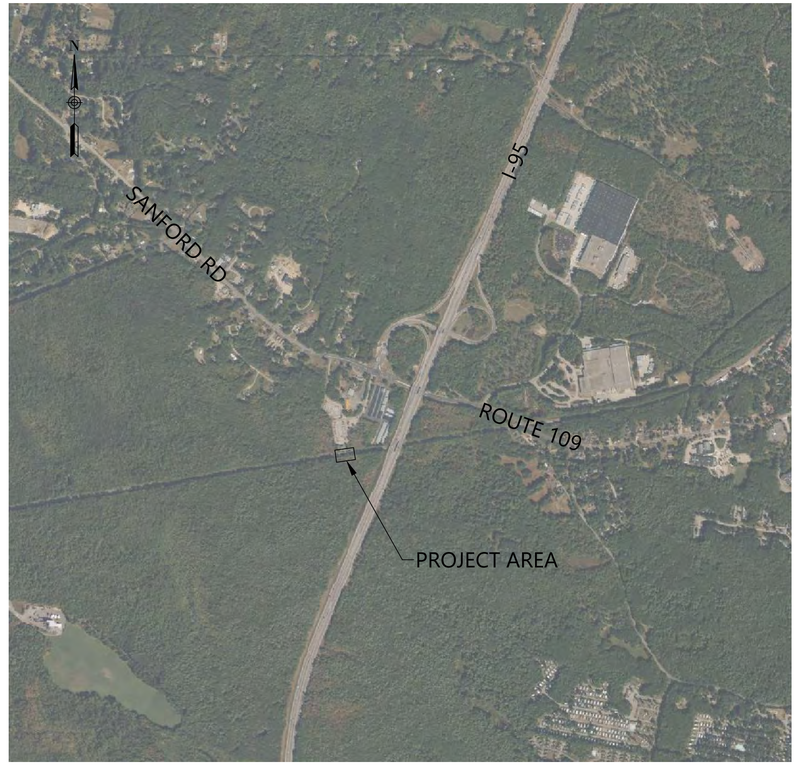
NORTHERN NEW ENGLAND PASSENGER RAIL AUTHORITY DATE

CSX CORPORATION DATE

FEDERAL RAILROAD ADMINISTRATION DATE

MAINE DEPARTMENT OF TRANSPORTATION DATE

AMTRAK DATE



0 1000 2000 3000 4000

SCALE: 1" = 1000'

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

COVER SHEET

SHEET NUMBER

G-001

Legend

Exist.	Prop.	
		PROPERTY LINE
		PROJECT LIMIT LINE
		RIGHT-OF-WAY/PROPERTY LINE
		EASEMENT
		BUILDING SETBACK
		PARKING SETBACK
10+00	10+00	BASELINE
		CONSTRUCTION LAYOUT
		ZONING LINE
		TOWN LINE
		LIMIT OF DISTURBANCE
		WETLAND LINE WITH FLAG
		FLOODPLAIN
BLSF	BLSF	BORDERING LAND SUBJECT TO FLOODING
BZ	BZ	WETLAND BUFFER ZONE
NDZ	NDZ	NO DISTURB ZONE
200'RA	200'RA	200' RIVERFRONT AREA
		GRAVEL ROAD
EOP	EOP	EDGE OF PAVEMENT
BB	BB	BITUMINOUS BERM
BC	BC	BITUMINOUS CURB
CC	CC	CONCRETE CURB
CG	CG	CURB AND GUTTER
ECC	ECC	EXTRUDED CONCRETE CURB
MCC	MCC	MONOLITHIC CONCRETE CURB
PCC	PCC	PRECAST CONC. CURB
SGE	SGE	SLOPED GRAN. EDGING
VGC	VGC	VERT. GRAN. CURB
		LIMIT OF CURB TYPE
		SAWCUT
		BUILDING
	EN	BUILDING ENTRANCE
	LD	LOADING DOCK
		BOLLARD
D	D	DUMPSTER PAD
		SIGN
		DOUBLE SIGN
		STEEL GUARDRAIL
		WOOD GUARDRAIL
		PATH
		TREE LINE
		WIRE FENCE
		FENCE
		STOCKADE FENCE
		STONE WALL
		RETAINING WALL
		STREAM / POND / WATER COURSE
		DETENTION BASIN
		HAY BALES
		SILT FENCE
		SILT SOCK / STRAW WATTLE
4	4	MINOR CONTOUR
20	20	MAJOR CONTOUR
10	10	PARKING COUNT
	C10	COMPACT PARKING STALLS
DYL	DYL	DOUBLE YELLOW LINE
SL	SL	STOP LINE
		CROSSWALK
		ACCESSIBLE CURB RAMP
		ACCESSIBLE PARKING
VAN	VAN	VAN-ACCESSIBLE PARKING

Exist.	Prop.	
		CONCRETE
		HEAVY DUTY PAVEMENT
		BUILDINGS
RIPRAP	RIPRAP	RIPRAP
		CONSTRUCTION EXIT
27.35 TC×	27.35 TC×	TOP OF CURB ELEVATION
26.85 BC×	26.85 BC×	BOTTOM OF CURB ELEVATION
132.75 ×	132.75 ×	SPOT ELEVATION
45.0 TW 38.5 BW	45.0 TW 38.5 BW	TOP & BOTTOM OF WALL ELEVATION
		BORING LOCATION
		TEST PIT LOCATION
MW	MW	MONITORING WELL
UD	UD	UNDERDRAIN
12"D	12"D	DRAIN
6"RD	6"RD	ROOF DRAIN
12"S	12"S	SEWER
FM	FM	FORCE MAIN
OHW	OHW	OVERHEAD WIRE
6"W	6"W	WATER
4"FP	4"FP	FIRE PROTECTION
	2"DW	DOMESTIC WATER
3"G	G	GAS
E	E	ELECTRIC
STM	STM	STEAM
T	T	TELEPHONE
FA	FA	FIRE ALARM
CATV	CATV	CABLE TV
		CATCH BASIN CONCENTRIC
		CATCH BASIN ECCENTRIC
		DOUBLE CATCH BASIN CONCENTRIC
		DOUBLE CATCH BASIN ECCENTRIC
		GUTTER INLET
		DRAIN MANHOLE CONCENTRIC
		DRAIN MANHOLE ECCENTRIC
TD		TRENCH DRAIN
		PLUG OR CAP
CO	CO	CLEANOUT
		FLARED END SECTION
		HEADWALL
S	S	SEWER MANHOLE CONCENTRIC
S	S	SEWER MANHOLE ECCENTRIC
CS	CS	CURB STOP & BOX
WV	WV	WATER VALVE & BOX
TSV	TSV	TAPPING SLEEVE, VALVE & BOX
		FIRE DEPARTMENT CONNECTION
HYD	HYD	FIRE HYDRANT
WM	WM	WATER METER
PIV	PIV	POST INDICATOR VALVE
W	W	WATER WELL
GG	GG	GAS GATE
GM	GM	GAS METER
E	EMH	ELECTRIC MANHOLE
EM	EM	ELECTRIC METER
		LIGHT POLE
T	TMH	TELEPHONE MANHOLE
T	T	TRANSFORMER PAD
		UTILITY POLE
		GUY POLE
		GUY WIRE & ANCHOR
HH	HH	HAND HOLE
PB	PB	PULL BOX
Match Line See Sheet C1.00		MATCHLINE

Abbreviations

General

ABAN	ABANDON
ACR	ACCESSIBLE CURB RAMP
ADJ	ADJUST
APPROX	APPROXIMATE
BIT	BITUMINOUS
BP	BOTTOM OF PLATFORM
BS	BOTTOM OF STAIRS
CONC	CONCRETE
DYCL	DOUBLE YELLOW CENTER LINE
EL/ELEV	ELEVATION
EX	EXISTING
FDN	FOUNDATION
FFE	FIRST FLOOR ELEVATION
FL	FLUSH
GRAN	GRANITE
GTD	GRADE TO DRAIN
LA	LANDSCAPE AREA
LOD	LIMIT OF DISTURBANCE
MAX	MAXIMUM
MIN	MINIMUM
NTS	NOT TO SCALE
PERF	PERFORATED
PROP	PROPOSED
REM	REMOVE
RET	RETAIN
R&D	REMOVE AND DISPOSE
R&R	REMOVE AND RESET
SWEL	SOLID WHITE EDGE LINE
SWLL	SOLID WHITE LANE LINE
TP	TOP OF PLATFORM
TR	TOP OF RAIL
TS	TOP OF STAIRS
TYP	TYPICAL

Utility

CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CO	CLEANOUT
DCB	DOUBLE CATCH BASIN
DMH	DRAIN MANHOLE
CIP	CAST IRON PIPE
COND	CONDUIT
DIP	DUCTILE IRON PIPE
FES	FLARED END SECTION
FM	FORCE MAIN
F&G	FRAME AND GRATE
F&C	FRAME AND COVER
GI	GUTTER INLET
HDPE	HIGH DENSITY POLYETHYLENE PIPE
HH	HANDHOLE
HW	HEADWALL
HYD	HYDRANT
INV	INVERT ELEVATION
I=	INVERT ELEVATION
LP	LIGHT POLE
PIV	POST INDICATOR VALVE
PWW	PAVED WATER WAY
PVC	POLYVINYLCHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
R=	RIM ELEVATION
RIM=	RIM ELEVATION
SMH	SEWER MANHOLE
TSV	TAPPING SLEEVE, VALVE AND BOX
UG	UNDERGROUND
UP	UTILITY POLE

Notes

Project Information

1. THIS IS A PROJECT OF THE NORTHERN NEW ENGLAND PASSENGER RAIL AUTHORITY (NNEPRA).
   PROJECT NAME: NNEPRA WELLS STATION EXPANSION
   LOCATION: WELLS, ME
2. PROJECT DESCRIPTION: THE MAJOR ELEMENTS OF THIS PROJECT INCLUDE, BUT ARE NOT LIMITED TO, REPLACEMENT OF EXISTING NORTH HIGH PLATFORM AND CANOPY, REPLACEMENT OF EXISTING NORTH LOW PLATFORM CANOPY, REHABILITATION OF EXISTING NORTH LOW PLATFORM, CONSTRUCTION OF A TEMPORARY NORTH HIGH PLATFORM, CONSTRUCTION OF NEW SOUTH HIGH AND LOW PLATFORMS AND CANOPIES, CONSTRUCTION OF NEW NORTH AND SOUTH STAIR AND ELEVATOR TOWERS AND PEDESTRIAN BRIDGE OVER THE TRACKS, CONSTRUCTION OF A SAFE DISPERSAL AREA ON THE SOUTH SIDE OF THE TRACKS AND ASSOCIATED WALKWAY, LIGHTING, DRAINAGE, SITE, UTILITY, SIGNAGE, AND OTHER IMPROVEMENTS AS SHOWN IN THESE PLANS AND THE OTHER CONTRACT DOCUMENTS.
3. EXISTING STATION BUILDING AND PARKING FACILITIES OWNER: MAINE TURNPIKE AUTHORITY
4. RAILROAD OWNER: CSX
5. EXISTING AND PROPOSED PLATFORMS OWNER: NNEPRA

General

1. CONTRACTOR SHALL NOTIFY "DIG-SAFE" (1-888-344-7233) AT LEAST 72 HOURS BEFORE EXCAVATING.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SECURITY AND JOB SAFETY. CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH OSHA STANDARDS AND LOCAL REQUIREMENTS.
3. ACCESSIBLE ROUTES, PARKING SPACES, RAMPS, SIDEWALKS AND WALKWAYS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE FEDERAL AMERICANS WITH DISABILITIES ACT AND WITH STATE AND LOCAL LAWS AND REGULATIONS (WHICHEVER ARE MORE STRINGENT).
4. AREAS DISTURBED DURING CONSTRUCTION AND NOT RESTORED WITH IMPERVIOUS SURFACES (BUILDINGS, PAVEMENTS, WALKS, ETC.) SHALL RECEIVE SIX INCHES LOAM AND SEED.
5. UPON AWARD OF CONTRACT, CONTRACTOR SHALL MAKE NECESSARY CONSTRUCTION NOTIFICATIONS AND APPLY FOR AND OBTAIN NECESSARY PERMITS, PAY FEES, AND POST BONDS ASSOCIATED WITH THE WORK INDICATED ON THE DRAWINGS, IN THE SPECIFICATIONS, AND IN THE CONTRACT DOCUMENTS. DO NOT CLOSE OR OBSTRUCT ROADWAYS, SIDEWALKS, AND FIRE HYDRANTS, WITHOUT APPROPRIATE PERMITS.
6. TRAFFIC SIGNAGE AND PAVEMENT MARKINGS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
7. AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
8. IN THE EVENT THAT SUSPECTED CONTAMINATED SOIL, GROUNDWATER, AND OTHER MEDIA ARE ENCOUNTERED DURING EXCAVATION AND CONSTRUCTION ACTIVITIES BASED ON VISUAL, OLFACTORY, OR OTHER EVIDENCE, THE CONTRACTOR SHALL STOP WORK IN THE VICINITY OF THE SUSPECT MATERIAL TO AVOID FURTHER SPREADING OF THE MATERIAL, AND SHALL NOTIFY THE OWNER IMMEDIATELY SO THAT THE APPROPRIATE TESTING AND SUBSEQUENT ACTION CAN BE TAKEN.
9. CONTRACTOR SHALL PREVENT DUST, SEDIMENT, AND DEBRIS FROM EXITING THE SITE AND SHALL BE RESPONSIBLE FOR CLEANUP, REPAIRS AND CORRECTIVE ACTION IF SUCH OCCURS.
10. DAMAGE RESULTING FROM CONSTRUCTION LOADS & ACTIVITIES SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.
11. CONTRACTOR SHALL CONTROL STORMWATER RUNOFF DURING CONSTRUCTION TO PREVENT ADVERSE IMPACTS TO OFF SITE AREAS, AND SHALL BE RESPONSIBLE TO REPAIR RESULTING DAMAGES, IF ANY, AT NO COST TO OWNER.

Utilities

1. THE LOCATIONS, SIZES, AND TYPES OF EXISTING UTILITIES ARE SHOWN AS AN APPROXIMATE REPRESENTATION ONLY. THE OWNER OR ITS REPRESENTATIVE(S) HAVE NOT INDEPENDENTLY VERIFIED THIS INFORMATION AS SHOWN ON THE PLANS. THE UTILITY INFORMATION SHOWN DOES NOT GUARANTEE THE ACTUAL EXISTENCE, SERVICEABILITY, OR OTHER DATA CONCERNING THE UTILITIES, NOR DOES IT GUARANTEE AGAINST THE POSSIBILITY THAT ADDITIONAL UTILITIES MAY BE PRESENT THAT ARE NOT SHOWN ON THE PLANS. PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY AND DETERMINE THE EXACT LOCATIONS, SIZES, AND ELEVATIONS OF THE POINTS OF CONNECTIONS TO EXISTING UTILITIES AND, SHALL CONFIRM THAT THERE ARE NO INTERFERENCES WITH EXISTING UTILITIES AND THE PROPOSED UTILITY ROUTES, INCLUDING ROUTES WITHIN THE PUBLIC RIGHTS OF WAY.
2. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, OR EXISTING CONDITIONS DIFFER FROM THOSE SHOWN SUCH THAT THE WORK CANNOT BE COMPLETED AS INTENDED, THE LOCATION, ELEVATION, AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED IN WRITING TO THE OWNER'S REPRESENTATIVE FOR THE RESOLUTION OF THE CONFLICT AND CONTRACTOR'S FAILURE TO NOTIFY PRIOR TO PERFORMING ADDITIONAL WORK RELEASES OWNER FROM OBLIGATIONS FOR ADDITIONAL PAYMENTS WHICH OTHERWISE MAY BE WARRANTED TO RESOLVE THE CONFLICT.
3. SET CATCH BASIN RIMS, AND INVERTS OF SEWERS, DRAINS, AND DITCHES IN ACCORDANCE WITH ELEVATIONS ON THE GRADING AND UTILITY PLANS.
4. RIM ELEVATIONS FOR DRAIN AND SEWER MANHOLES, WATER VALVE COVERS, GAS GATES, ELECTRIC AND TELEPHONE PULL BOXES, AND MANHOLES, AND OTHER SUCH ITEMS, ARE APPROXIMATE AND SHALL BE SET/RESET AS FOLLOWS:
   A. PAVEMENTS AND CONCRETE SURFACES: FLUSH
   B. ALL SURFACES ALONG ACCESSIBLE ROUTES: FLUSH
   C. LANDSCAPE, LOAM AND SEED, AND OTHER EARTH SURFACE AREAS: ONE INCH ABOVE SURROUNDING AREA AND TAPER EARTH TO THE RIM ELEVATION.
5. THE LOCATION, SIZE, DEPTH, AND SPECIFICATIONS FOR CONSTRUCTION OF PROPOSED PRIVATE UTILITY SERVICES SHALL BE INSTALLED ACCORDING TO THE REQUIREMENTS PROVIDED BY, AND APPROVED BY, THE RESPECTIVE UTILITY COMPANY (GAS, TELEPHONE, ELECTRIC, FIRE ALARM, ETC.). FINAL DESIGN LOADS AND LOCATIONS SHALL BE COORDINATED WITH OWNER AND ARCHITECT.
6. CONTRACTOR SHALL MAKE ARRANGEMENTS FOR AND SHALL BE RESPONSIBLE FOR PAYING FEES FOR POLE RELOCATION AND FOR THE ALTERATION AND ADJUSTMENT OF ELECTRIC, TELEPHONE, FIRE ALARM, AND ANY OTHER PRIVATE UTILITIES, WHETHER WORK IS PERFORMED BY CONTRACTOR OR BY THE UTILITIES COMPANY.
7. UTILITY PIPE MATERIALS SHALL BE AS FOLLOWS, UNLESS OTHERWISE NOTED ON THE PLANS:
   A. STORM DRAINAGE PIPES SHALL BE HDPE UNLESS OTHERWISE NOTED.
   B. PIPE INSTALLATION AND MATERIALS SHALL COMPLY WITH THE STATE PLUMBING CODE WHERE APPLICABLE.
8. CONTRACTOR SHALL COORDINATE WITH ELECTRICAL CONTRACTOR AND SHALL FURNISH EXCAVATION, INSTALLATION, AND BACKFILL OF ELECTRICAL FURNISHED SITEWORK RELATED ITEMS SUCH AS PULL BOXES, CONDUITS, DUCT BANKS, LIGHT POLE BASES, AND CONCRETE PADS. SITE CONTRACTOR SHALL FURNISH CONCRETE ENCASEMENT OF DUCT BANKS IF REQUIRED BY THE UTILITY COMPANY AND AS INDICATED ON THE DRAWINGS.
9. ALL DRAINAGE AND SANITARY STRUCTURE INTERIOR DIAMETERS (4' MIN.) SHALL BE DETERMINED BY THE MANUFACTURER BASED ON THE PIPE CONFIGURATIONS SHOWN ON THESE PLANS AND LOCAL MUNICIPAL STANDARDS. FOR MANHOLES THAT ARE 20 FEET IN DEPTH AND GREATER, THE MINIMUM DIAMETER SHALL BE 5 FEET.

Typical ADA Slopes

1. THE CONTRACTOR SHALL USE THE FOLLOWING CRITERIA AT ALL NEW PAVING AREAS, WHICH HAVE BEEN ADJUSTED BY THE DESIGNERS TO ALLOW FOR FIELD TOLERANCES.
2. WALKWAYS
   2.1. MAX SLOPE AT LANDINGS SHALL NOT EXCEED 1:60 (1.67%) IN ANY DIRECTION
   2.2. MAX SLOPE AT RAMPED WALKWAYS SHALL NOT EXCEED 1:25 (4%).
   2.3. CROSS SLOPES SHALL NOT EXCEED 1:60 (1.67%) IN ANY DIRECTION.
3. RAMPS AND LANDINGS
   3.1. MAX SLOPE AT LANDINGS SHALL NOT EXCEED 1:60 (1.67%) IN ANY DIRECTION.
   3.2. MAX SLOPE AT RAMPS SHALL NOT EXCEED (7.69%).
   3.3. RAMP CROSS SLOPES SHALL NOT EXCEED 1:60 (1.67%).
4. PLATFORMS
   4.1. FRONT EDGE OF LOW PLATFORMS SHALL BE SET 8" ABOVE TOP OF RAIL.
   4.2. MAX SLOPE OF PLATFORMS FROM FRONT EDGE TO BACK EDGE SHALL NOT EXCEED 1:96 (1.04%).
   4.3. CROSS SLOPE OF PLATFORMS SHALL BE SET BY EXISTING TOP OF RAIL ELEVATIONS; MAXIMUM CROSS SLOPE SHALL NOT EXCEED 1:60 (1.67%).
5. MAX SLOPE AT ADA PARKING STALLS SHALL NOT EXCEED 1:60 (1.67%) IN ANY DIRECTION
6. ADJUST DIMENSIONS AS REQUIRED IN THE FIELD SO THAT EDGES AT ALL TRANSITIONS BETWEEN NEW AND EXISTING PAVEMENTS SHALL BE FULL THICKNESS AND SET FLUSH WITH EXISTING PAVEMENT AND MEET THE ABOVE CRITERIA.
7. IF THESE TOLERANCES CANNOT BE ACHIEVED DUE TO EXISTING CONDITIONS DISCREPANCIES, CONTRACTOR SHALL NOTIFY PROJECT ENGINEER IMMEDIATELY.

Layout and Materials

1. DIMENSIONS ARE FROM THE FACE OF CURB, FACE OF BUILDING, FACE OF WALL, AND CENTER LINE OF PAVEMENT MARKINGS, UNLESS OTHERWISE NOTED.
2. SEE ARCHITECTURAL DRAWINGS FOR EXACT BUILDING DIMENSIONS AND DETAILS CONTIGUOUS TO THE BUILDING, INCLUDING SIDEWALKS, RAMPS, BUILDING ENTRANCES, STAIRWAYS, UTILITY PENETRATIONS, CONCRETE DOOR PADS, ETC.
3. PROPOSED BOUNDS AND ANY EXISTING PROPERTY LINE MONUMENTATION DISTURBED DURING CONSTRUCTION SHALL BE SET OR RESET BY A PROFESSIONAL LAND SURVEYOR.
4. PRIOR TO START OF CONSTRUCTION, CONTRACTOR SHALL VERIFY EXISTING PAVEMENT ELEVATIONS AT INTERFACE WITH PROPOSED PAVEMENTS, AND EXISTING GROUND ELEVATIONS ADJACENT TO DRAINAGE OUTLETS TO ASSURE PROPER TRANSITIONS BETWEEN EXISTING AND PROPOSED FACILITIES.

Demolition

1. CONTRACTOR SHALL REMOVE AND DISPOSE OF EXISTING MANMADE SURFACE FEATURES WITHIN THE LIMIT OF WORK INCLUDING BUILDINGS, STRUCTURES, PAVEMENTS, SLABS, CURBING, FENCES, UTILITY POLES, SIGNS, ETC. AS INDICATED ON THE DRAWINGS. REMOVE AND DISPOSE OF EXISTING UTILITIES, FOUNDATIONS AND UNSUITABLE MATERIAL BENEATH AND FOR A DISTANCE OF 10 FEET BEYOND THE FOOTPRINT OF PROPOSED STRUCTURES.
2. EXISTING UTILITIES SHALL BE TERMINATED, UNLESS OTHERWISE NOTED. IN CONFORMANCE WITH LOCAL, STATE AND INDIVIDUAL UTILITY COMPANY STANDARD SPECIFICATIONS AND DETAILS. THE CONTRACTOR SHALL COORDINATE UTILITY SERVICE DISCONNECTS WITH THE UTILITY REPRESENTATIVES.
3. CONTRACTOR SHALL DISPOSE OF DEMOLITION DEBRIS IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS, ORDINANCES AND STATUTES.
4 THE DEMOLITION LIMITS DEPICTED IN THE PLANS ARE INTENDED TO AID THE CONTRACTOR DURING THE BIDDING AND CONSTRUCTION PROCESS AND ARE NOT INTENDED TO DEPICT EACH AND EVERY ELEMENT OF DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE DETAILED SCOPE OF DEMOLITION BEFORE SUBMITTING ITS BID/PROPOSAL TO PERFORM THE WORK .
5. UNLESS OTHERWISE SPECIFICALLY PROVIDED ON THE PLANS OR IN THE SPECIFICATIONS, THE ENGINEER HAS NOT PREPARED DESIGNS FOR AND SHALL HAVE NO RESPONSIBILITY FOR THE PRESENCE, DISCOVERY, REMOVAL, ABATEMENT OR DISPOSAL OF HAZARDOUS MATERIALS, TOXIC WASTES OR POLLUTANTS AT THE PROJECT SITE. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR ANY CLAIMS OF LOSS, DAMAGE, EXPENSE, DELAY, INJURY OR DEATH ARISING FROM THE PRESENCE OF HAZARDOUS MATERIAL AND CONTRACTOR SHALL INDEMNIFY AND HOLD HARMLESS THE ENGINEER FROM ANY CLAIMS MADE IN CONNECTION THEREWITH.

Existing Conditions Information

1. BASE PLAN: THE PROPERTY LINES SHOWN WERE DETERMINED BY AN ACTUAL FIELD SURVEY CONDUCTED BY VHB IN OCTOBER 2022, AND FROM PLANS OF RECORD. THE TOPOGRAPHY AND PHYSICAL FEATURES ARE BASED ON AN ACTUAL FIELD SURVEY PERFORMED ON THE GROUND BY VHB, DURING OCTOBER 2022.
   A. DELINEATION OF THE WETLANDS AND PLACEMENT OF THE FLAGS WAS PERFORMED BY: VHB.
   B. FLAGS MARKING THE WETLANDS WERE LOCATED BY: VHB.
2. TOPOGRAPHY: ELEVATIONS ARE BASED ON NAVD 88 DATUM.
3. GEOTECHNICAL DATA INCLUDING TEST PIT AND BORING LOCATIONS AND ELEVATIONS WERE OBTAINED FROM GZA ENVIRONMENTAL.

Railroad Requirements

1. ALL ACTIVITIES WITHIN 25 FT FROM THE CENTERLINE OF THE NEAREST TRACK ARE CONSIDERED FOULING THE TRACK. DURING CONSTRUCTION, CLEAR THE FOUL AREA BY REMOVING ALL PERSONNEL AND EQUIPMENT A MINIMUM OF 25 FT FROM THE CENTERLINE OF THE NEAREST TRACK DURING TRAIN OPERATION. NOTICE FOR CLEARING THE FOUL AREA DURING TRAIN OPERATIONS WILL BE GIVEN BY AN ONSITE RAILROAD PROTECTION FLAGMAN. CLEAR THE FOUL AREA AND REMAIN CLEAR OF THE FOUL AREA AND REMAIN CLEAR OF THE FOUL AREA UNTIL ALLOWED TO RETURN AS INSTRUCTED BY THE FLAGMAN.
2. PLACEMENT OR STORAGE OF MATERIAL OR EQUIPMENT WILL NOT BE PERMITTED WITHIN 25 FEET FROM CENTERLINE OF AN ADJACENT TRACK, EXCEPT AS MAY BE NOTED IN THE APPROVED SITE SPECIFIC WORK PLAN. TO ENSURE THIS REQUIREMENT, ESTABLISH A 25 FT LINE PRIOR TO THE START OF WORK BY DRIVING STAKES, TAPING OFF, OR ERECTING A TEMPORARY FENCE.
3. CONFORM TO CSX GUIDELINES FOR TEMPORARY SHORING.
4. SUBMIT SITE SPECIFIC WORK PLANS (SSWP) INCLUDING COMPUTATIONS AND A DETAILED DESCRIPTION OF PROPOSED METHODS FOR ACCOMPLISHING THE WORK, INCLUDING METHODS FOR PROTECTING HOST RAILROAD TRAFFIC. MULTIPLE SSWP MAY BE REQUIRED AS DIRECTED BY THE PROJECT ENGINEER, DEPENDENT UPON THE WORK TASKS AND DURATIONS OF EACH WORK TASK.
5. NO CELL PHONE OR RADIO USE IS ALLOWED WITHIN 25 FT OF THE TRACK CENTERLINE.
6. COORDINATE ALL SIGN INSTALLATION LOCATION WITH THE HOST RAILROAD AND AMTRAK TO ADDRESS OPERATIONS CONCERNS, CONTRACTOR SHALL NOTIFY MAINE ONE CALL CENTER.
7. STAGE THE WORK AS TO MAINTAIN THE NORMAL TRAIN OPERATIONS AND ACCESS FOR PASSENGER MOVEMENT FOR BOARDING AND DEPARTING THE TRAINS AND ACCESS TO ADJACENT PARKING FACILITIES.
8. THE FLAGGING ACTIVITIES AND PRESENCE WILL BE DETERMINED BY THE RAILROAD AND WILL BE BASED UPON THE APPROVED SITE SPECIFIC WORK PLAN.
9. COORDINATE WITH RAILROAD TO LOCATE, PROTECT, AND RESTORE ALL BURIED UTILITIES, SIGNAL AND COMMUNICATION CABLES. VERIFY AND MARK ALL UTILITIES, SIGNAL, AND COMMUNICATION CABLES PRIOR TO ANY EXCAVATION. IN ADDITION TO MARK CUTS PERFORMED FOR, OR BY THE UTILITY COMPANIES NOTIFY THE HOST RAILROAD FOR A SEPARATE DIG TICKET PRIOR TO EXCAVATION.
10. CONTRACT DOCUMENTS SHALL NOT BE SCALED. IF DIMENSIONS ARE MISSING, COORDINATE THROUGH SHOP DRAWINGS.
11. EXAMINE THE SITE AND CONDITIONS AND REVIEW THE CONSTRUCTION DOCUMENTS PRIOR TO STARTING WORK. ANY DISCREPANCIES SHALL BE REPORTED TO THE PROJECT ENGINEER IN WRITING PRIOR TO COMMENCEMENT OF WORK.

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

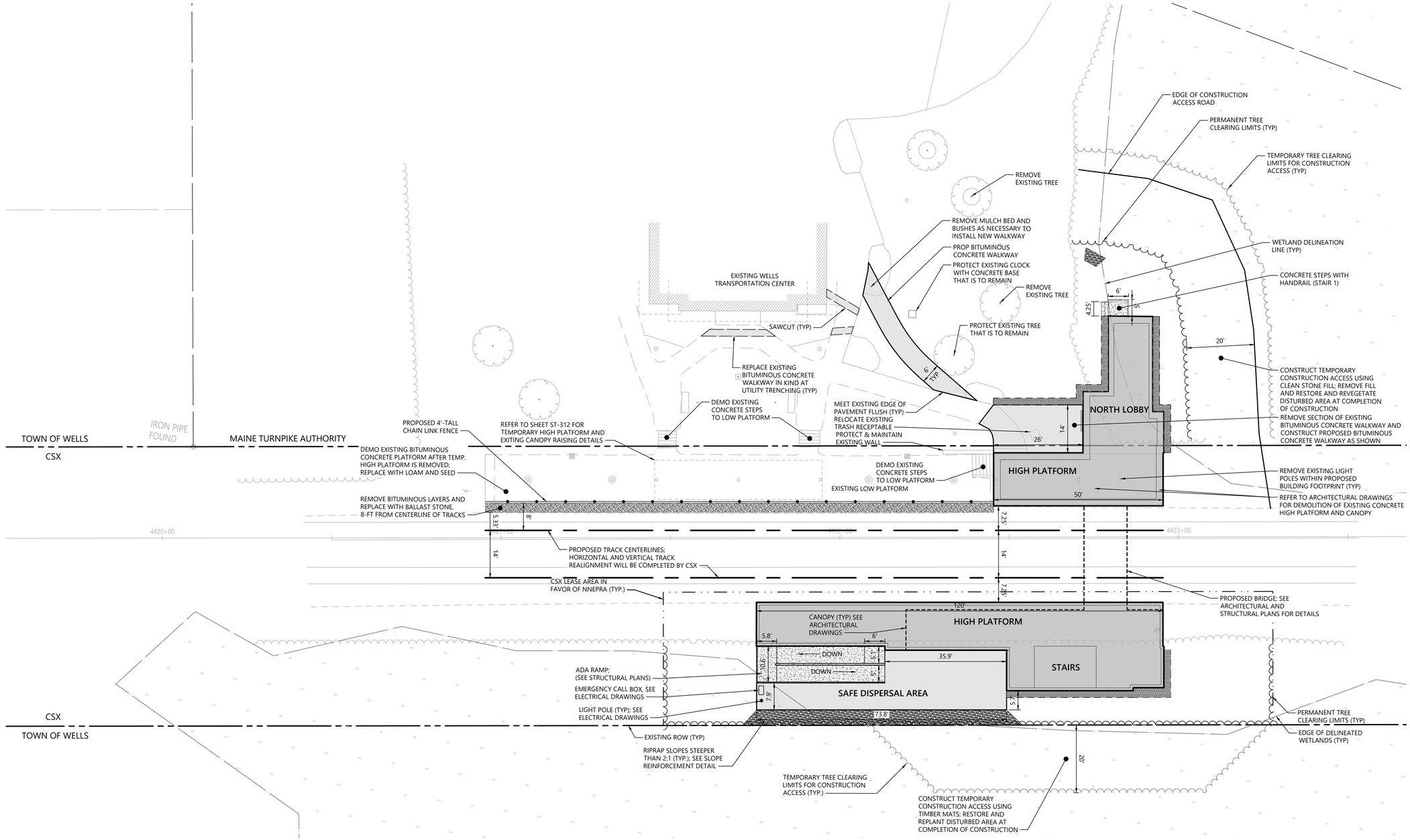
PROJECT INFORMATION	
DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL LEGEND AND GENERAL NOTES

SHEET NUMBER

C-101



NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION	
DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
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WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL SITE PLAN

SHEET NUMBER

C-102

TOWN OF WELLS

CSX

IRON PIPE FOUND

MAINE TURNPIKE AUTHORITY

CONTRACTOR TO LOCATE STABILIZED CONSTRUCTION EXIT AT EDGE OF PAVEMENT WHERE ACCESS IS PERMITTED

GRADE SLOPE TO MEET EXISTING (SEE SECTION A-A ON SHEET C-106)

DOUBLE ROW OF PERIMETER CONTROLS REQUIRED WHEN WITHIN 75' OF A PROTECTED RESOURCE (TYP)

RIP RAP STONE PROTECTION INV=129.2

103 FES INV=129.20 102

UNDERGROUND ELECTRICAL AND FIBER CONDUIT; REFER TO ELECTRICAL DRAWINGS FOR DETAILS

REFER TO ELECTRICAL DRAWINGS FOR HANDHOLE LOCATIONS AND ELECTRICAL DETAILS & EXTENSION TO PARKING LOT AREA

102 DMH RIM=134.80 INV=129.35 101 INV=129.25 OUT

PROVIDE NEW DMH OVER END OF EXISTING PIPE WHERE CUT

101 DMH RIM=135.75 INV=129.38 OUT

TRENCH DRAIN (BY ARCHITECT); CONNECT TO UNDERDRAIN

GRADE AT 2:1 SLOPE TO MEET EXISTING (APPROXIMATE SLOPE SHOWN)

CONNECT 4" PERFORATED UNDERDRAIN TO DMH-2

4" PERFORATED HDPE UNDERDRAIN PIPE

2'-WIDE DRIP EDGE TRENCH WITH UNDERDRAIN; SEE DETAIL

REMOVE SECTION OF EXISTING 24" RCP

CLEANOUT (TYP)

SEE PLUMBING PLANS FOR ELEVATOR PIT DISCHARGE DETAILS (TYP); OUTLET TO DISCHARGE TO GRADE

EXISTING CCTV LINE; SEE ARCHITECTURAL & STRUCTURAL PLANS FOR RELOCATION THROUGH NEW STRUCTURE

CONTRACTOR TO COORDINATE WITH CSX & FLAGGING ON EROSION CONTROLS NEAR TRACKS

MEET EXISTING EDGE OF PAVEMENT FLUSH (TYP)

NORTH LOBBY

FFE = 136.57

HIGH PLATFORM

EXISTING BURIED AT&T FIBER OPTIC LINE TO BE PROTECTED & MAINTAINED DURING CONSTRUCTION; CONTRACTOR TO COORDINATE WITH AT&T

EXISTING SPRINT/COGENT FIBER OPTIC LINE TO REMAIN; FIELD LOCATED APPROX. 3" NORTH OF OF THE EDGE OF PLATFORM; CONTRACTOR TO COORDINATE WITH UTILITY PROVIDER ON PROTECTING FIBER OPTIC LINE IN PLACE

CSX LEASE AREA IN FAVOR OF NNEPRA (TYP.)

SEE PLUMBING PLANS FOR ELEVATOR PIT DISCHARGE DETAILS (TYP); OUTLET TO DISCHARGE TO GRADE

HIGH PLATFORM

STAIRS

SAFE DISPERSAL AREA

LIGHT POLE (TYP); SEE ELECTRICAL DRAWINGS

1.5:1 SLOPE

PROVIDE DAYLIGHT FOR UNDERDRAIN WHERE POSSIBLE (TYP)

CSX

TOWN OF WELLS

0 15 30 Feet

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

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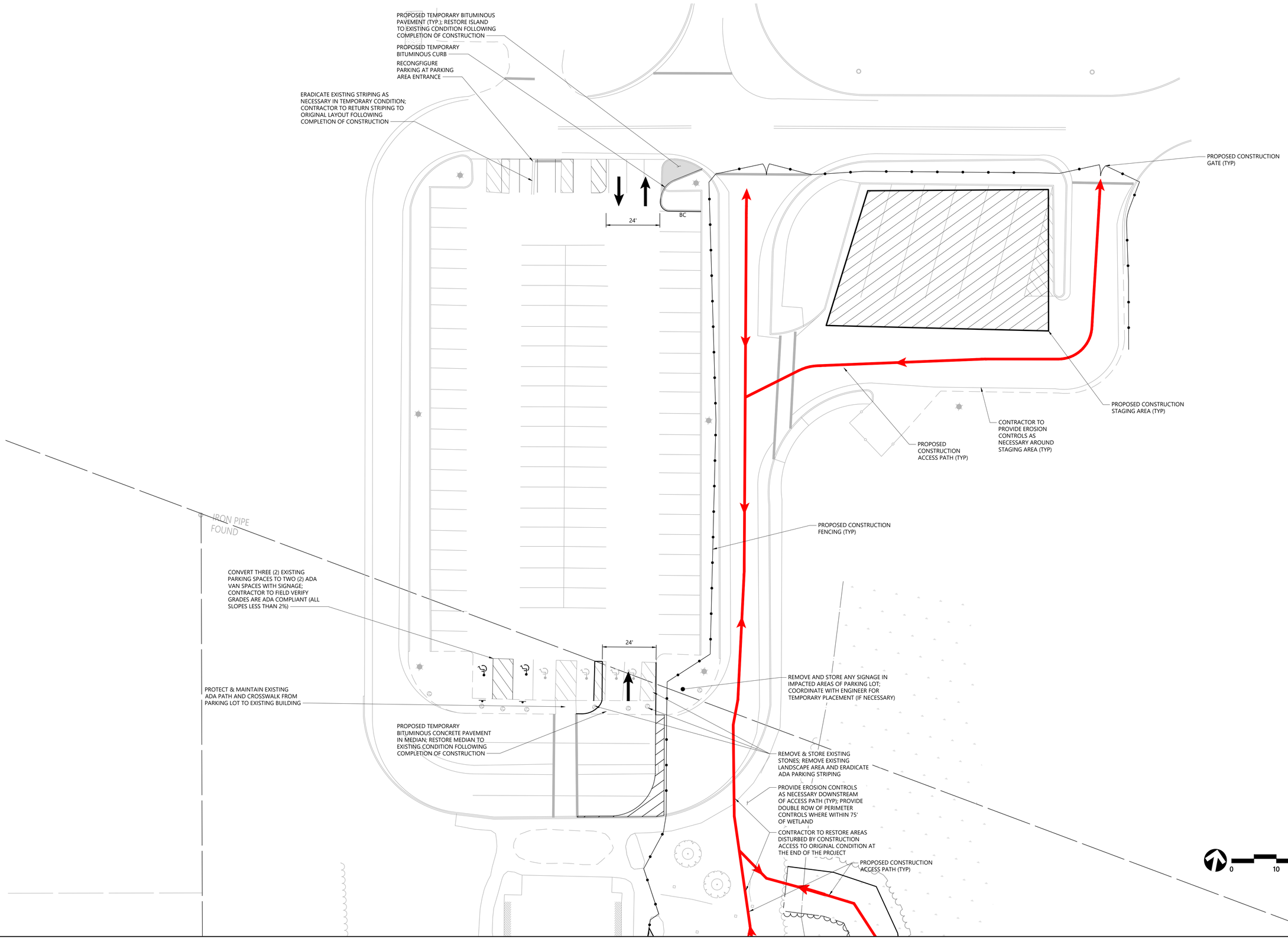
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL SITE GRADING, EROSION CONTROL AND UTILITY PLAN

SHEET NUMBER

C-103



NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
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REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CONSTRUCTION STAGING & ACCESS PLAN

SHEET NUMBER

C-104

NOTES

1. CONSTRUCTION ACCESS ROUTE AND STAGING AREA ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR IS RESPONSIBLE FOR FINAL CONSTRUCTION ACCESS AND STAGING.

PROPOSED CONSTRUCTION ACCESS PATH (TYP)

20'-WIDE SECTION FOR TEMPORARY CONSTRUCTION ACCESS THROUGH WETLAND; RESTORE AREA TO EXISTING CONDITIONS FOLLOWING CONSTRUCTION

PROPOSED CONSTRUCTION FENCING (TYP.)

NORTH LOBBY

EN

HIGH PLATFORM

4422+00

4423+00

4424+00

CONTRACTOR SHALL COORDINATE WITH CSX FOR CONSTRUCTION ACCESS ACROSS THE TRACKS TO THE SOUTH PORTION OF PROJECT

HIGH PLATFORM

STAIRS

SAFE DISPERSAL AREA

120'

20'

80'

PROPOSED TEMPORARY CONSTRUCTION ACCESS AREA (TYP)

0 10 20 Feet

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

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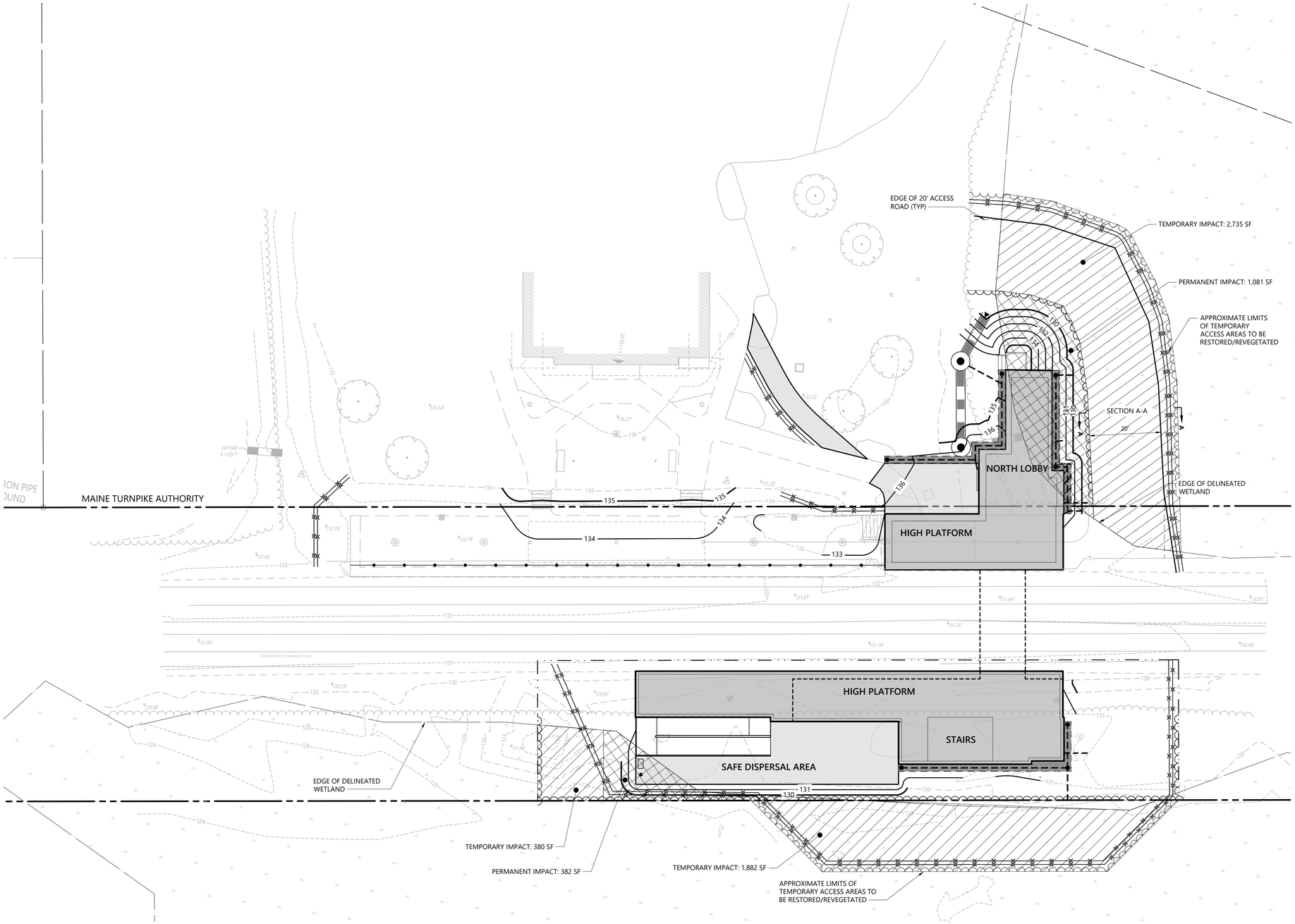
REVISIONS 5	
PROJECT COMPLETION DATE	

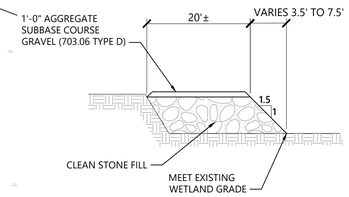
WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CONSTRUCTION STAGING & ACCESS PLAN

SHEET NUMBER

C-105





Temp. Access Typical Section (Section A-A) 11/23

N.T.S. Source: VHB

Wetland Impact Summary

Permanent Wetland Impact		1,463 SF
Temporary Wetland Impact		4,997 SF
Total Wetland Impact		6,460 SF

0 15 30 Feet

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

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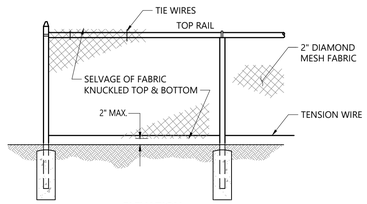
WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

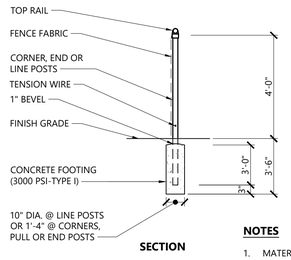
WETLAND IMPACT PLAN

SHEET NUMBER

C-106



ELEVATION



SECTION

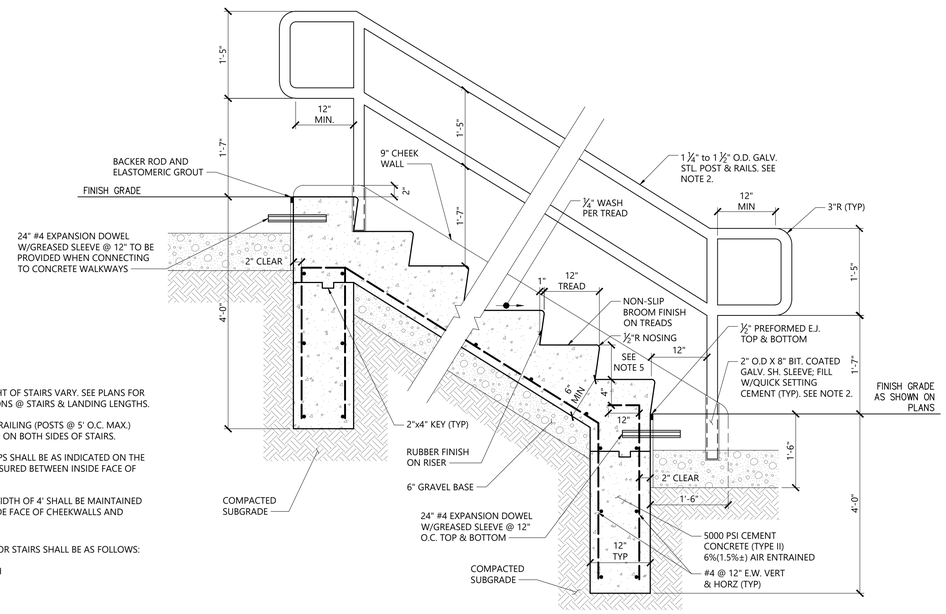
NOTES

1. MATERIALS TO BE SUPPLIED AND INSTALLED IN CONFORMANCE WITH "CHAIN LINK MANUFACTURER'S INSTITUTE" PRODUCT MANUAL.

4' Chain Link Fence

10/20

N.T.S. Source: VHB LD_481



NOTES

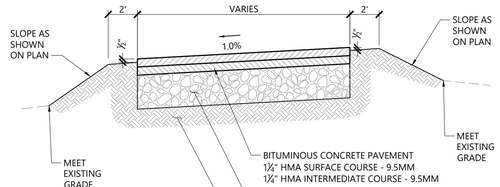
1. SPACE & HEIGHT OF STAIRS VARY. SEE PLANS FOR SPOT ELEVATIONS @ STAIRS & LANDING LENGTHS.
2. CONTINUOUS RAILING (POSTS @ 5' O.C. MAX.) SHALL BE USED ON BOTH SIDES OF STAIRS.
3. WIDTH OF STEPS SHALL BE AS INDICATED ON THE PLANS AS MEASURED BETWEEN INSIDE FACE OF CHEEK WALLS.
4. A MINIMUM WIDTH OF 4' SHALL BE MAINTAINED BETWEEN INSIDE FACE OF CHEEKWALLS AND HANDRAILS.
5. RISER DEPTH FOR STAIRS SHALL BE AS FOLLOWS:

   STAIR 1: 6 INCH

Concrete Steps with Handrail

11/15

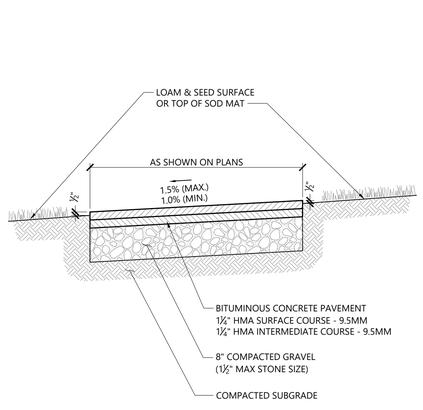
N.T.S. Source: VHB REV LD_765



Safe Dispersal Area Typical Section

11/15

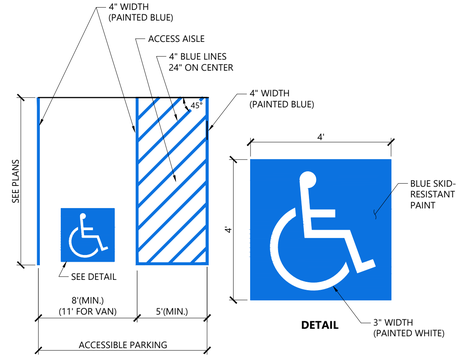
N.T.S. Source: VHB LD_



Bituminous Concrete Sidewalk in Landscape Area

3/21

N.T.S. Source: VHB REV LD_426



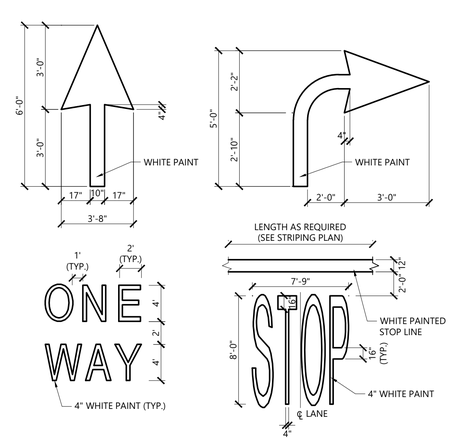
NOTES

1. ALL DIMENSIONS TO CENTER OF 4" PAVEMENT STRIPING.
2. ALL SLOPES THROUGHOUT THE ACCESSIBLE PARKING AND AISLE AREAS SHALL NOT EXCEED 1.5%.

Accessible Parking Space

12/19

N.T.S. Source: VHB LD_552A



NOTES

1. PAVEMENT MARKINGS TO BE INSTALLED FOR ON SITE WORK IN LOCATIONS SHOWN.

Painted Pavement Markings - On Site

1/16

N.T.S. Source: VHB LD_554

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION	
DATE	12/11/2023
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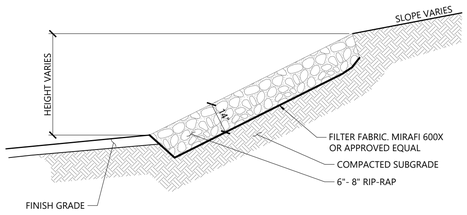
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL SITE DETAILS

SHEET NUMBER

C-107



NOTES

1. SLOPE TO BE FOUNDED ON UNDISTURBED MATERIAL OR GRAVEL AND COMPACTED CONSISTENT WITH GEOTECHNICAL ENGINEER'S RECOMMENDATIONS.
2. DESIGN SUBJECT TO CHANGE BASED ON REVIEW BY GEOTECHNICAL ENGINEER

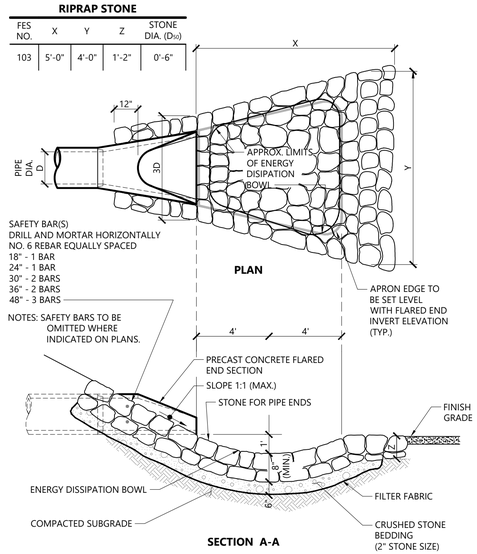
Rip-Rap Reinforced Slope

10/20

N.T.S. Source: VHB LD_760

RIPRAP STONE

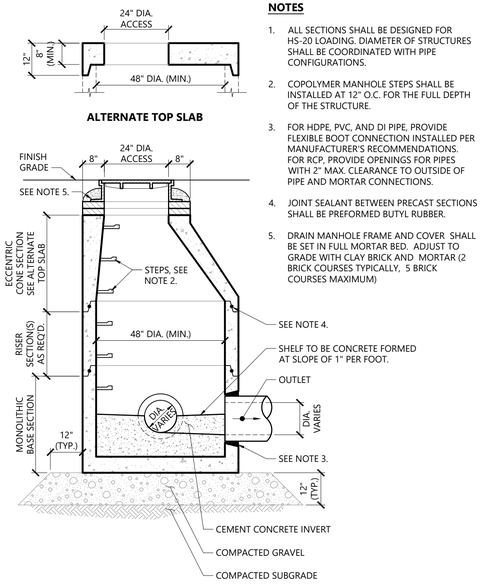
FES NO.	X	Y	Z	STONE DIA. (D_{50})
103	5'-0"	4'-0"	1'-2"	0'-6"



Flared End Section (FES) with Stone Protection

3/19

N.T.S. Source: VHB LD_134



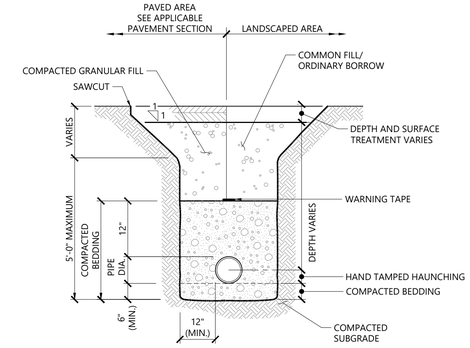
NOTES

1. ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING. DIAMETER OF STRUCTURES SHALL BE COORDINATED WITH PIPE CONFIGURATIONS.
2. COPOLYMER MANHOLE STEPS SHALL BE INSTALLED AT 12" O.C. FOR THE FULL DEPTH OF THE STRUCTURE.
3. FOR HDPE, PVC, AND DI PIPE, PROVIDE FLEXIBLE BOOT CONNECTION INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. FOR RCP, PROVIDE OPENINGS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE AND MORTAR CONNECTIONS.
4. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PREFORMED BUTYL RUBBER.
5. DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM)

Drain Manhole (DMH)

11/19

N.T.S. Source: VHB LD_115



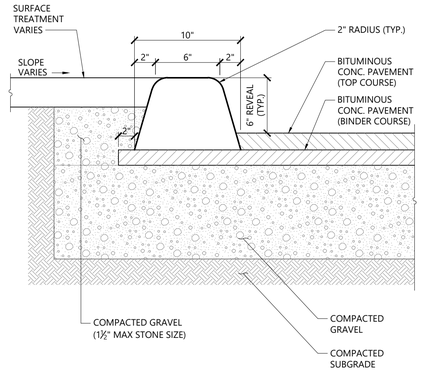
NOTES

1. WHERE UTILITY TRENCHES ARE CONSTRUCTED THROUGH DETENTION BASIN BERMS OR OTHER SUCH SPECIAL SECTIONS, PLACE TRENCH BACKFILL WITH MATERIALS SIMILAR TO THE SPECIAL SECTION REQUIREMENTS.
2. USE METALLIC TRACING/WARNING TAPE OVER ALL PIPES.
3. COMPACTED GRANULAR FILL MAY CONSIST OF GRAVEL, CRUSHED STONE, SAND, OR OTHER MATERIAL AS APPROVED BY ENGINEER.

Utility Trench

11/19

N.T.S. Source: VHB LD_300



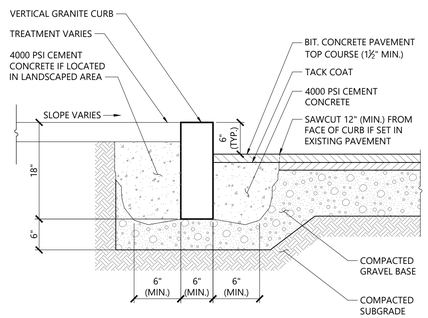
NOTES

ALL CURBING TO BE MACHINE EXTRUDED

Bituminous Curb (BC)

1/16

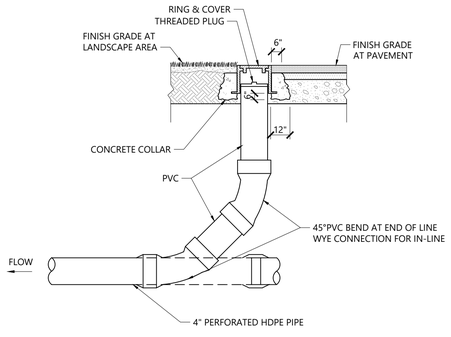
N.T.S. Source: VHB LD_406



Vertical Granite Curb (VGC)

3/20

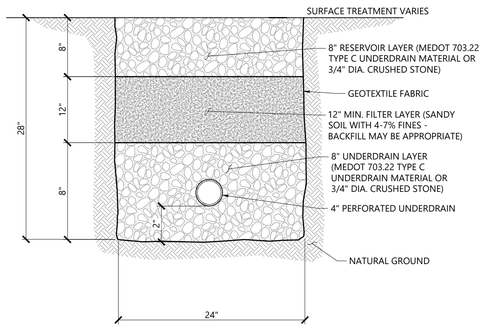
N.T.S. Source: VHB LD_402



Cleanout (CO)

12/19

N.T.S. Source: VHB LD_303



NOTES

1. DO NOT TRAFFIC EXPOSED SOIL SURFACE WITH CONSTRUCTION EQUIPMENT. IF FEASIBLE, PERFORM EXCAVATIONS WITH EQUIPMENT POSITIONED OUTSIDE THE LIMITS OF THE SYSTEM.
2. DO NOT PLACE SYSTEMS INTO SERVICE UNTIL THE CONTRIBUTING AREAS HAVE BEEN FULLY STABILIZED.

Drip Edge Trench (With Underdrain)

04/23

N.T.S. Source: VHB REV

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL SITE DETAILS

SHEET NUMBER

C-108

General

1. CONTRACTOR SHALL READ, BE FAMILIAR WITH, AND SHALL FOLLOW THE MAINE EROSION AND SEDIMENT CONTROL BMPS MANUAL (LATEST EDITION) AND MAINE EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONTRACTORS (LATEST EDITION); AND SHALL BE ACCOUNTABLE TO THE THIRD PARTY INSPECTOR FOR THE PROJECT AND THE MAINE DEP IN ACCORDANCE WITH MAINE DEP REGULATIONS.

2. PRIOR TO STARTING ANY OTHER WORK ON THE SITE, THE CONTRACTOR SHALL NOTIFY APPROPRIATE AGENCIES AND SHALL INSTALL TEMPORARY EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS IDENTIFIED IN THE CONTRACT DOCUMENTS.

3. CONTRACTOR SHALL BE FULLY RESPONSIBLE TO CONTROL CONSTRUCTION SUCH THAT SEDIMENTATION SHALL NOT AFFECT REGULATORY PROTECTED AREAS, WHETHER SUCH SEDIMENTATION IS CAUSED BY WATER, WIND, OR DIRECT DEPOSIT.

4. MINIMUM TEMPORARY AND PERMANENT EROSION AND SEDIMENTATION CONTROL MEASURES ARE SHOWN ON THE EROSION AND SEDIMENTATION CONTROL PLAN. THE CONTRACTOR SHALL ADHERE TO THE MINIMUM PROVISIONS SHOWN. ADDITIONALLY, TEMPORARY MEASURES SHALL BE SELECTED AND CONSTRUCTED BY THE CONTRACTOR IN CONSULTATION WITH THE ENGINEER TO ACCOMMODATE CHANGING FIELD CONDITIONS THAT DEVELOP DURING CONSTRUCTION.

5. PUMPED WATER FROM DEWATERING ACTIVITIES SHALL BE DISCHARGED INTO SETTLING BASINS, FILTER BAGS OR OTHER APPROVED METHODS PRIOR TO DISCHARGE INTO THE ON-SITE STORMWATER MANAGEMENT SYSTEM. ALL WATER FROM DEWATERING ACTIVITIES SHALL BE RECHARGED ON-SITE OR DIRECTED TO THE DETENTION BASIN FOR DISCHARGE.

6. NO MORE THAN 1 ACRE SHOULD BE UNSTABILIZED AT ONE TIME WITHOUT REGULAR INSPECTION OR LIMITED TO AN AREA THAT CAN BE MULCHED IN ONE DAY.

Seeding/Mulching

1. FERTILIZER, SUPERPHOSPHATE, AND LIME SHALL BE APPLIED AT RATES APPROVED BY THE ENGINEER.

2. PERMANENT SEED SHALL BE SUPPLIED IN THE FOLLOWING PROPORTIONS AND APPLIED AT A RATE OF FIVE POUNDS PER 1,000 SF:
   SEED TYPE (% PROPORTION/% GERMINATION MIN./% PURITY MIN.)
   CREEPING FESCUE (50/85/95)
   KENTUCKY BLUEGRASS (40/85/90)
   MANHATTAN PERENNIAL RYE (10/90/95)

3. TEMPORARY SEED SHALL BE SUPPLIED IN THE FOLLOWING PROPORTIONS AND APPLIED AT A RATE OF 100 POUNDS PER ACRE:
   SEED TYPE (% WEIGHT MIN./% GERMINATION MIN.)
   WINTER RYE (80/85)
   RED FESCUE - CREEPING (4/80)
   PERENNIAL RYE GRASS (3/90)
   RED CLOVER (3/90)

4. MULCH SHALL BE APPLIED TO AREAS IMMEDIATELY AFTER THEY HAVE BEEN SEEDED. MULCH SHALL CONSIST OF HAY, STRAW, HYDRO-MULCH, EROSION CONTROL BLANKETS, EROSION CONTROL MIX OR APPROVED EQUAL.

5. HAY OR STRAW MULCH SHALL BE AIR-DRIED; AND FREE OF UNDESIRABLE SEEDS AND COARSE MATERIALS. MULCH SHALL BE APPLIED AT A MINIMUM RATE OF 75 LB PER 1,000 SF. MULCH SHALL BE ANCHORED WITH NETTING WHEN APPLIED TO SLOPES GREATER THAN 15 PERCENT.

6. EROSION CONTROL BLANKETS SHALL BE PROVIDED ON ALL SLOPES STEEPER THAN OF 1-FOOT RISE TO 3-FEET HORIZONTAL. BLANKETS SHALL BE SC150 BN (NORTH AMERICAN GREEN); CURLEX BLANKETS (AMERICAN EXCELSIOR COMPANY); POLYJUTE STYLE 465 GT (SYNTHETIC INDUSTRIES); OR APPROVED EQUIVALENT. BLANKETS SHALL BE SECURED AS RECOMMENDED BY THE MANUFACTURER.

7. EROSION CONTROL MIX SHALL MEET THE FOLLOWING STANDARDS:
   A. ORGANIC MATTER CONTENT SHALL BE BETWEEN 80%-100%, DRY WEIGHT BASIS,
   B. PARTICLE SIZE BY WEIGHT: 100% PASSING THE 6" SCREEN
      70% TO 85% PASSING THE 0.75" SCREEN
   C. ORGANIC PORTION SHALL BE FIBROUS AND ELONGATED
   D. SOLUBLE SALTS CONTENT SHALL BE < 4.0 MMHOS/CM, AND
   E. pH SHALL BE BETWEEN 5.0 AND 8.0.

Temporary Erosion Control Measures

1. CONTRACTOR SHALL PERFORM CONSTRUCTION SEQUENCING SUCH THAT EARTH MATERIALS ARE EXPOSED FOR A MINIMUM AMOUNT OF TIME BEFORE THEY ARE COVERED, SEEDED, OR OTHERWISE STABILIZED TO PREVENT EROSION. AREAS REMAINING UNSTABILIZED FOR A PERIOD OF MORE THAN 15 DAYS SHALL BE TEMPORARILY MULCHED. TOTAL EXPOSED AREAS SHALL BE LIMITED TO NO MORE THAN CAN BE MULCHED IN ONE DAY.

2. TEMPORARY MULCH SHALL BE APPLIED TO UNSTABILIZED AREAS WITHIN 100-FT OF STREAMS, WETLANDS, AND OTHER WATER RESOURCES WITHIN 7 DAYS OF EXPOSING SOIL AND PRIOR TO ANY STORM EVENT.

3. DUST SHALL BE CONTROLLED THROUGH THE USE OF WATER.

4. CONTRACTOR SHALL PROVIDE TEMPORARY SILTATION/DEWATERING BASINS, IF NECESSARY AND/OR AS DIRECTED BY THE ENGINEER, TO CONTROL SEDIMENTATION AND STORMWATER RUNOFF DURING THE CONSTRUCTION PERIOD. CONTRACTOR SHALL SUBMIT PROPOSED BASIN LOCATIONS, DESIGNS, ETC. TO THE ENGINEER FOR REVIEW PRIOR TO CONSTRUCTION.

5. EARTH MATERIAL STOCKPILES SHALL BE LOCATED IN AREAS THAT HAVE A MINIMUM POTENTIAL FOR EROSION AND KEPT AS FAR AWAY AS POSSIBLE FROM EXISTING DRAINAGE COURSES, PROTECTED NATURAL RESOURCES, TREE DRIP LINES AND OUTSIDE OF THE 100-YEAR FLOOD PLAIN. SEDIMENT BARRIERS SHALL BE INSTALLED DOWNGRADIENT OF STOCKPILES. STORMWATER SHOULD BE DIRECTED AWAY FROM STOCKPILE LOCATIONS.

6. REPAIR, CLEAN, AND REPLACE ANY SEDIMENT CONTROLS DAMAGED DURING AND/OR AFTER RAINFALL EVENTS.

7. EROSION CONTROL BLANKETS SHALL BE PLACED IN THE FLOW LINE OF ALL VEGETATED SWALES NOT OTHERWISE PROTECTED BY STONE.

8. EROSION CONTROL BLANKETS OR NETTING OVER LOOSE MULCH SHALL BE APPLIED TO ALL VEGETATED SLOPES GREATER THAN 3:1.

9. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:

   A. BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;

   B. A MINIMUM OF 90% VEGETATED GROWTH HAS BEEN ESTABLISHED;

   C. A MINIMUM OF 3-INCHES OF NON-EROSIVE MATERIAL, SUCH AS STONE OR RIPRAP, HAS BEEN INSTALLED;

   D. EROSION CONTROL BLANKETS OR EROSION CONTROL MIX HAVE BEEN PROPERLY INSTALLED.

Permanent Erosion Control Measures

1. SEEDING SHALL BE DONE BETWEEN APRIL 1 TO JUNE 1, OR BETWEEN AUGUST 15 TO OCTOBER 15.

2. ALL DISTURBED AREAS NOT COVERED BY BUILDINGS, PAVING, OR OTHERWISE DEVELOPED, SHALL BE COVERED WITH 6 INCHES LOAM AND SEEDED.

Winter Construction

1. WINTER CONSTRUCTION PERIOD: OCTOBER 15 THRU APRIL 15.

2. WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED SUCH THAT A MAXIMUM OF 1 ACRE OF THE SITE IS UNSTABILIZED AT ANY ONE TIME OR LIMITED TO AN AREA THAT CAN BE MULCHED IN ONE DAY.

3. HAY AND STRAW MULCH SHALL BE APPLIED AT A RATE OF 150 LB PER 1,000 SF OR 3 TONS/ACRE. MULCH SHALL BE APPLIED AND ANCHORED SO THAT THE GROUND SURFACE IS NOT VISIBLE THROUGHOUT THE MULCH. MULCH SHALL NOT BE APPLIED OVER SNOW.

4. MULCH SHALL NOT BE APPLIED WHERE THE SNOW DEPTH EXCEEDS ONE INCH. SNOW SHALL BE REMOVED PRIOR TO APPLICATION.

5. EROSION CONTROL BLANKETS SHALL BE APPLIED TO ALL VEGETATED SLOPES GREATER THAN 3:1.

6. A DOUBLE ROW OF SEDIMENT BARRIERS SHALL BE INSTALLED WITHIN 75 FEET OF A PROTECTED NATURAL RESOURCE.

7. DURING PERIODS WHEN TEMPERATURES ARE ABOVE FREEZING, AREAS SHALL BE FINE GRADED AND PROTECTED WITH EITHER MULCH; OR TEMPORARILY SEEDED AND MULCHED UNTIL THE FINAL TREATMENT CAN BE APPLIED.

8. AFTER NOVEMBER 1 EXPOSED AREAS THAT HAVE BEEN LOAMED AND FINAL GRADED MAY BE DORMANT SEEDED AT A RATE OF 3 TIME THE PERMANENT SEED RATE AFTER THE FIRST KILLING FROST AND OVERWINTER MULCHED OR ANCHORED WITH EROSION CONTROL BLANKETS.

9. WINTER INSPECTIONS SHALL BE PERFORMED ONE A WEEK AND AFTER EACH RAINFALL, SNOWSTORM, OR THAW FOR VEGETATION GROWTH, EROSION, AND MAINTENANCE NEEDS.

   A. ALL AREAS INSUFFICIENTLY VEGETATED (LESS THAN 75% CATCH) SHALL BE STABILIZED FOR OVERWINTER PROTECTION.

Site Inspection & Maintenance

1. CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS AND BEFORE AND AFTER EACH STORM EVENT.

2. CONTRACTOR SHALL MAINTAIN WRITTEN INSPECTION AND MAINTENANCE LOGS FOR THE EROSION CONTROL MEASURES FOR THE DURATION OF THE CONSTRUCTION PERIOD. LOGS SHALL BE MADE AVAILABLE TO THE OWNER, ENGINEER, MUNICIPALITY, RAILROAD, AND MAINE DEP UPON REQUEST.

3. TEMPORARY MULCHING: ADDITIONAL MULCH SHALL BE IMMEDIATELY APPLIED TO AREAS WHERE LESS THAN 90% OF THE SOIL SURFACE IS COVERED WITH MULCH.

4. CATCH BASIN/SILT SACK SEDIMENT TRAPS: SEDIMENT SHALL BE REMOVED FROM TRAPS WHEN ACCUMULATION DEPTH IS GREATER THAN OR EQUAL TO 1/2 THE DESIGN DEPTH OF THE TRAP. TRAPS SHALL BE REPLACED IF THEY ARE DAMAGED, TORN, ETC.

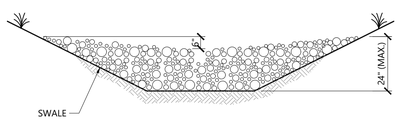
5. SILTSOCK BARRIERS, SILT FENCE BARRIERS, AND STONE CHECK DAMS: SILTSOCK BARRIERS, SILT FENCE, AND STONE CHECK DAMES SHALL BE REPAIRED IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THEM. SEDIMENT TRAPPED BEHIND BARRIERS/CHECK DAM SHALL BE REMOVED WHEN SEDIMENT DEPTH REACHES 6 INCHES. BARRIERS SHALL BE REPLACES WITH A TEMPORARY CHECK DAM IF THERE ARE SIGNS OF UNDERCUTTING OR IMPOUNDING LARGE VOLUMES OF WATER BEHIND THEM.

6. EROSION CONTROL BLANKETS: IF WASHOUTS OR BREAKAGE OCCURS, SLOPES SHALL BE REPAIRED, AND BLANKETS SHALL BE RE-INSTALLED.

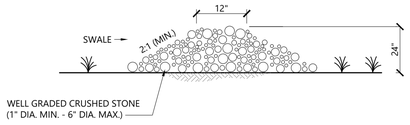
7. STABILIZED CONSTRUCTION EXITS: EXITS SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. IF EXIT BECOMES INEFFECTIVE IT SHALL BE RECONSTRUCTED AND/OR REPLACED.

8. TEMPORARY SEDIMENTATION/DEWATERING BASINS: SEDIMENT IN TEMPORARY BASINS SHALL BE REMOVED AS NECESSARY DEPENDING ON THEIR USE AND DESIGN.

9. UPON COMPLETION OF CONSTRUCTION AND ESTABLISHMENT OF PERMANENT GROUND COVER, CONTRACTOR SHALL REMOVE AND DISPOSE OF EROSION CONTROL MEASURES AND CLEAN SEDIMENT AND DEBRIS FROM ENTIRE DRAINAGE SYSTEMS.



ELEVATION



CROSS-SECTION

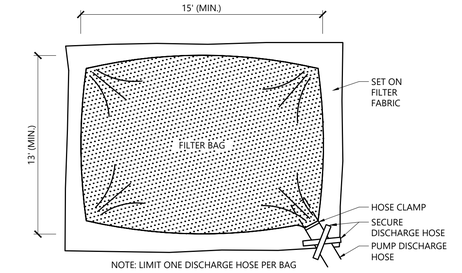
NOTES

1. TOP OF DOWNGRADIENT CHECKDAM AND BOTTOM OF UPGRADIENT CHECKDAM TO BE SET AT THE SAME ELEVATION.

2. STONE CHECKDAMS MAY BE REMOVED WHEN 90% OF THE VEGETATIVE COVER IS ESTABLISHED.

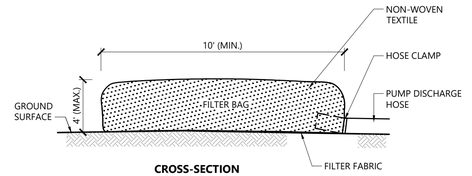
Temporary Stone Checkdam

N.T.S. Source: VHB REV



NOTE: LIMIT ONE DISCHARGE HOSE PER BAG

PLAN VIEW



CROSS-SECTION

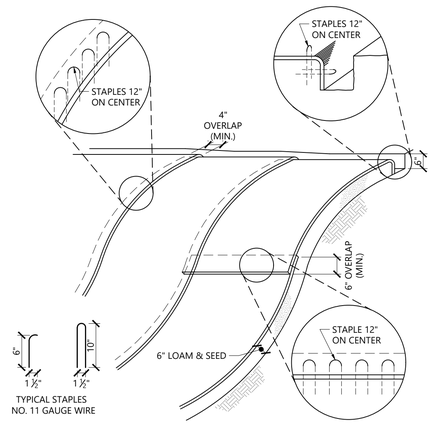
NOTES

1. BAG TO BE USED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.

Dewatering Filter Bag

1/16

N.T.S. Source: VHB REV LD_691



TYPICAL STAPLES
NO. 11 GAUGE WIRE

NOTES

1. BEGIN AT THE TOP OF BLANKET INSTALLATION AREA BY ANCHORING BLANKET IN A 6" DEEP TRENCH BACKFILL AND COMPACT TRENCH AFTER STAPLING.

2. ROLL THE BLANKET DOWN THE SWALE IN THE DIRECTION OF THE WATER FLOW.

3. THE EDGES OF BLANKETS MUST BE STAPLED WITH APPROX. 4 INCH OVERLAP WHERE 2 OR MORE STRIP WIDTHS ARE REQUIRED.

4. WHEN BLANKETS MUST BE SPLICED DOWN THE SWALE, PLACE UPPER BLANKET END OVER LOWER END WITH 6 INCH (MIN.) OVERLAP AND STAPLE BOTH TOGETHER.

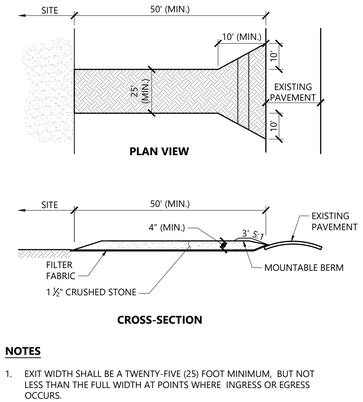
5. METHOD OF INSTALLATION SHALL BE AS PER MANUFACTURER'S RECOMMENDATIONS.

6. EROSION CONTROL BLANKETS SHALL BE USED IN ALL AREAS WHERE SLOPES EXCEED 3:1.

Erosion Control Blanket Slope Installation

1/16

N.T.S. Source: VHB REV LD_680



PLAN VIEW

CROSS-SECTION

NOTES

1. EXIT WIDTH SHALL BE A TWENTY-FIVE (25) FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.

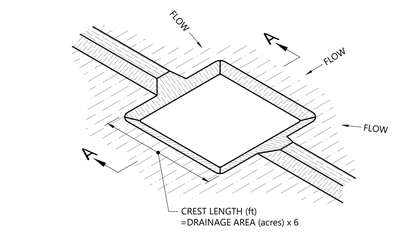
2. THE EXIT SHALL BE MAINTAINED IN A CONDITION WHICH SHALL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. BERM SHALL BE PERMITTED. PERIODIC INSPECTION AND MAINTENANCE SHALL BE PROVIDED AS NEEDED.

3. STABILIZED CONSTRUCTION EXIT SHALL BE REMOVED PRIOR TO FINAL FINISH MATERIALS BEING INSTALLED.

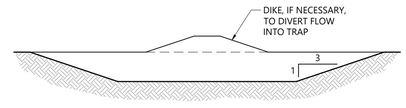
Stabilized Construction Exit

1/16

N.T.S. Source: VHB LD_682



ISOMETRIC VIEW



SECTION A-A

NOTES

1. THE TRAP SHALL BE INSTALLED AS CLOSE TO THE DISTURBED AREA OR SOURCE OF SEDIMENT AS POSSIBLE .

2. THE MAXIMUM CONTRIBUTING DRAINAGE AREA TO THE TRAP SHALL BE LESS THAN 5 ACRES.

3. THE MINIMUM VOLUME OF THE TRAP SHALL BE 3,600 CUBIC FEET OF STORAGE FOR EACH ACRE OF DRAINAGE AREA.

4. THE SIDE SLOPES OF THE TRAP SHALL BE 3:1 OR FLATTER, AND SHALL BE STABILIZED IMMEDIATELY AFTER THEIR CONSTRUCTION.

5. THE OUTLET OF THE TRAP SHALL BE A MINIMUM OF ONE FOOT BELOW THE CREST OF THE TRAP AND SHALL DISCHARGE TO A STABILIZED AREA.

6. THE TRAP SHALL BE CLEANED WHEN 50 PERCENT OF THE ORIGINAL VOLUME IS FILLED.

7. THE MATERIALS REMOVED FROM THE TRAP SHALL BE PROPERLY DISPOSED OF AND STABILIZED.

Temporary Sediment Trap

N.T.S. Source: NH Stormwater Manual

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

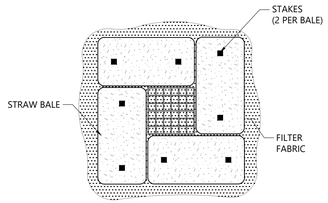
DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

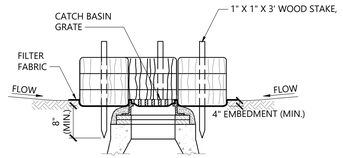
CIVIL SITE DETAILS

SHEET NUMBER

C-109



PLAN VIEW



SECTION VIEW

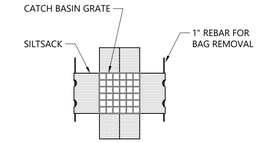
NOTES

1. ENCLOSE STRUCTURE WITH HAYBALES IMMEDIATELY AFTER CATCH BASIN CONSTRUCTION. MAINTAIN UNTIL PAVING BINDER COURSE IS COMPLETE OR A PERMANENT STAND OF GRASS HAS BEEN ESTABLISHED.
2. IF GRATE IS AGAINST EXISTING CURB THEN BALES ARE TO BE PLACED AROUND THREE SIDES OF GRATE ONLY.
3. GRATE TO BE PLACED OVER FILTER FABRIC.
4. BALES SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED.

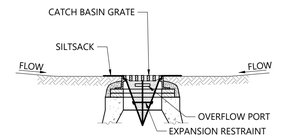
Catch Basin Sediment Trap

1/16

N.T.S. | Source: VHB | LD_673



PLAN VIEW



SECTION VIEW

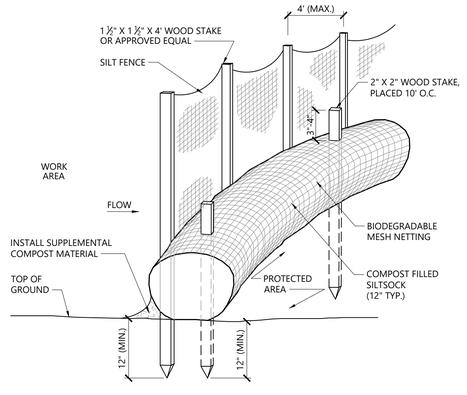
NOTES

1. INSTALL SILTSACK IN ALL CATCH BASINS WHERE INDICATED ON THE PLAN BEFORE COMMENCING WORK OR IN PAVED AREAS AFTER BINDER COURSE IS PLACED AND HAY BALES HAVE BEEN REMOVED.
2. GRATE TO BE PLACED OVER SILTSACK.
3. SILTSACK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND CLEANING OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED. MAINTAIN UNTIL UPSTREAM AREAS HAVE BEEN PERMANENTLY STABILIZED

Siltsack Sediment Trap

1/20

N.T.S. | Source: VHB | LD_674



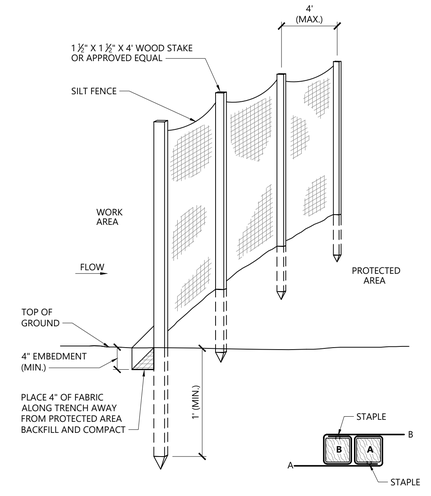
NOTES

1. SILTSOCK SHALL BE FILTREXX SILTSOXX, OR APPROVED EQUAL.
2. SILTSOCKS SHALL OVERLAP A MINIMUM OF 12 INCHES.
3. SILTSOCK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS, AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED.
4. COMPOST MATERIAL SHALL BE DISPERSED ON SITE, AS DETERMINED BY THE ENGINEER.
5. IF NON BIODEGRADABLE NETTING IS USED THE NETTING SHALL BE COLLECTED AND DISPOSED OF OFFSITE.

Siltsock / Silt Fence Barrier

1/16

N.T.S. | Source: VHB | REV | LD_658-A



WOOD STAKE JOINT DETAIL

Silt Fence Barrier

1/16

N.T.S. | Source: VHB | REV | LD_650

NNEPRA DOWNEASTER
WELLS AREA IMPROVEMENTS PROJECT
WELLS, MAINE

PROJECT INFORMATION

DATE	12/11/2023
DESIGNER	VHB
RAILROAD OWNER	CSX
REVISIONS 1	
REVISIONS 2	
REVISIONS 3	
REVISIONS 4	
REVISIONS 5	
PROJECT COMPLETION DATE	

WELLS TRANSPORTATION CENTER
WELLS STATION EXPANSION

CIVIL SITE DETAILS

SHEET NUMBER

C-110

Site Plans

Issued for: Permitting

Date Issued: December 7, 2023

Latest Issue: December 7, 2023

Wells Station Expansion - Wetland Restoration Plan

Wells Transportation Center, Wells, Maine

vhb.com

Civil Engineer/Surveyor/Wetland Scientist
VHB
2 Bedford Farms Drive
Suite 200
Bedford, NH 03110
603.391.3900

Applicant/Owner

NNEPRA
75 West Commercial Street, Suite 104
Portland, ME 04101

Sheet Index

No.	Drawing Title	Latest Issue
C-1.1	Overall Restoration Plan	12/7/2023
C-1.2	Restoration Planting Plan	12/7/2023
C-2.1	Site Details	12/7/2023

Reference Drawings

No.	Drawing Title	Latest Issue
Sv-1	Existing Conditions Plan	10/13/2022

GENERAL NOTES:

1. CONTRACTOR SHALL NOTIFY "DIG-SAFE" (1-888-344-7233) AT LEAST 72 HOURS BEFORE EXCAVATING.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SECURITY AND JOB SAFETY. CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH OSHA STANDARDS AND LOCAL REQUIREMENTS.
3. ACCESSIBLE ROUTES, PARKING SPACES, RAMPS, SIDEWALKS AND WALKWAYS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE FEDERAL AMERICANS WITH DISABILITIES ACT AND WITH STATE AND LOCAL LAWS AND REGULATIONS (WHICHEVER ARE MORE STRINGENT).
4. UPON AWARD OF CONTRACT, CONTRACTOR SHALL MAKE NECESSARY CONSTRUCTION NOTIFICATIONS AND APPLY FOR AND OBTAIN NECESSARY PERMITS, PAY FEES, AND POST BONDS ASSOCIATED WITH THE WORK INDICATED ON THE DRAWINGS, IN THE SPECIFICATIONS, AND IN THE CONTRACT DOCUMENTS. DO NOT CLOSE OR OBSTRUCT ROADWAYS, SIDEWALKS, AND FIRE HYDRANTS, WITHOUT APPROPRIATE PERMITS.
5. AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
6. IN THE EVENT THAT SUSPECTED CONTAMINATED SOIL, GROUNDWATER, AND OTHER MEDIA ARE ENCOUNTERED DURING EXCAVATION AND CONSTRUCTION ACTIVITIES BASED ON VISUAL, OLFACTORY, OR OTHER EVIDENCE, THE CONTRACTOR SHALL STOP WORK IN THE VICINITY OF THE SUSPECT MATERIAL TO AVOID FURTHER SPREADING OF THE MATERIAL, AND SHALL NOTIFY THE OWNER IMMEDIATELY SO THAT THE APPROPRIATE TESTING AND SUBSEQUENT ACTION CAN BE TAKEN.
7. CONTRACTOR SHALL PREVENT DUST, SEDIMENT, AND DEBRIS FROM EXITING THE SITE AND SHALL BE RESPONSIBLE FOR CLEANUP, REPAIRS AND CORRECTIVE ACTION IF SUCH OCCURS.
8. DAMAGE RESULTING FROM CONSTRUCTION LOADS & ACTIVITIES SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.
9. CONTRACTOR SHALL CONTROL STORMWATER RUNOFF DURING CONSTRUCTION TO PREVENT ADVERSE IMPACTS TO OFF SITE AREAS, AND SHALL BE RESPONSIBLE TO REPAIR RESULTING DAMAGES, IF ANY, AT NO COST TO OWNER.

EXISTING CONDITIONS NOTES:

1. BASE PLAN: THE PROPERTY LINES SHOWN WERE DETERMINED BY AN ACTUAL FIELD SURVEY CONDUCTED BY VHB IN OCTOBER 2022, AND FROM PLANS OF RECORD. THE TOPOGRAPHY AND PHYSICAL FEATURES ARE BASED ON AN ACTUAL FIELD SURVEY PERFORMED ON THE GROUND BY VHB, DURING OCTOBER 2022.
   A. DELINEATION OF THE WETLANDS AND PLACEMENT OF THE FLAGS WAS PERFORMED BY: VHB.
   B. FLAGS MARKING THE WETLANDS WERE LOCATED BY: VHB.
2. TOPOGRAPHY: ELEVATIONS ARE BASED ON NAVD 88 DATUM.
3. GEOTECHNICAL DATA INCLUDING TEST PIT AND BORING LOCATIONS AND ELEVATIONS WERE OBTAINED FROM GZA ENVIRONMENTAL.

TEMPORARY EROSION CONTROL MEASURES:

1. CONTRACTOR SHALL PERFORM CONSTRUCTION SEQUENCING SUCH THAT EARTH MATERIALS ARE EXPOSED FOR A MINIMUM AMOUNT OF TIME BEFORE THEY ARE COVERED, SEEDED, OR OTHERWISE STABILIZED TO PREVENT EROSION. AREAS REMAINING UNSTABILIZED FOR A PERIOD OF MORE THAN 15 DAYS SHALL BE TEMPORARILY MULCHED. TOTAL EXPOSED AREAS SHALL BE LIMITED TO NO MORE THAN CAN BE MULCHED IN ONE DAY.
2. TEMPORARY MULCH SHALL BE APPLIED TO UNSTABILIZED AREAS WITHIN 100-FT OF STREAMS, WETLANDS, AND OTHER WATER RESOURCES WITHIN 7 DAYS OF EXPOSING SOIL AND PRIOR TO ANY STORM EVENT.
3. DUST SHALL BE CONTROLLED THROUGH THE USE OF WATER.
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6. REPAIR, CLEAN, AND REPLACE ANY SEDIMENT CONTROLS DAMAGED DURING AND/OR AFTER RAINFALL EVENTS.
7. EROSION CONTROL BLANKETS SHALL BE PLACED IN THE FLOW LINE OF ALL VEGETATED SWALES NOT OTHERWISE PROTECTED BY STONE.
8. EROSION CONTROL BLANKETS OR NETTING OVER LOOSE MULCH SHALL BE APPLIED TO ALL VEGETATED SLOPES GREATER THAN 3:1.
9. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:
   A. BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
   B. A MINIMUM OF 90% VEGETATED GROWTH HAS BEEN ESTABLISHED;
   C. A MINIMUM OF 3-INCHES OF NON-EROSIVE MATERIAL, SUCH AS STONE OR RIPRAP, HAS BEEN INSTALLED;
   D. EROSION CONTROL BLANKETS OR EROSION CONTROL MIX HAVE BEEN PROPERLY INSTALLED.

PERMANENT EROSION CONTROL MEASURES

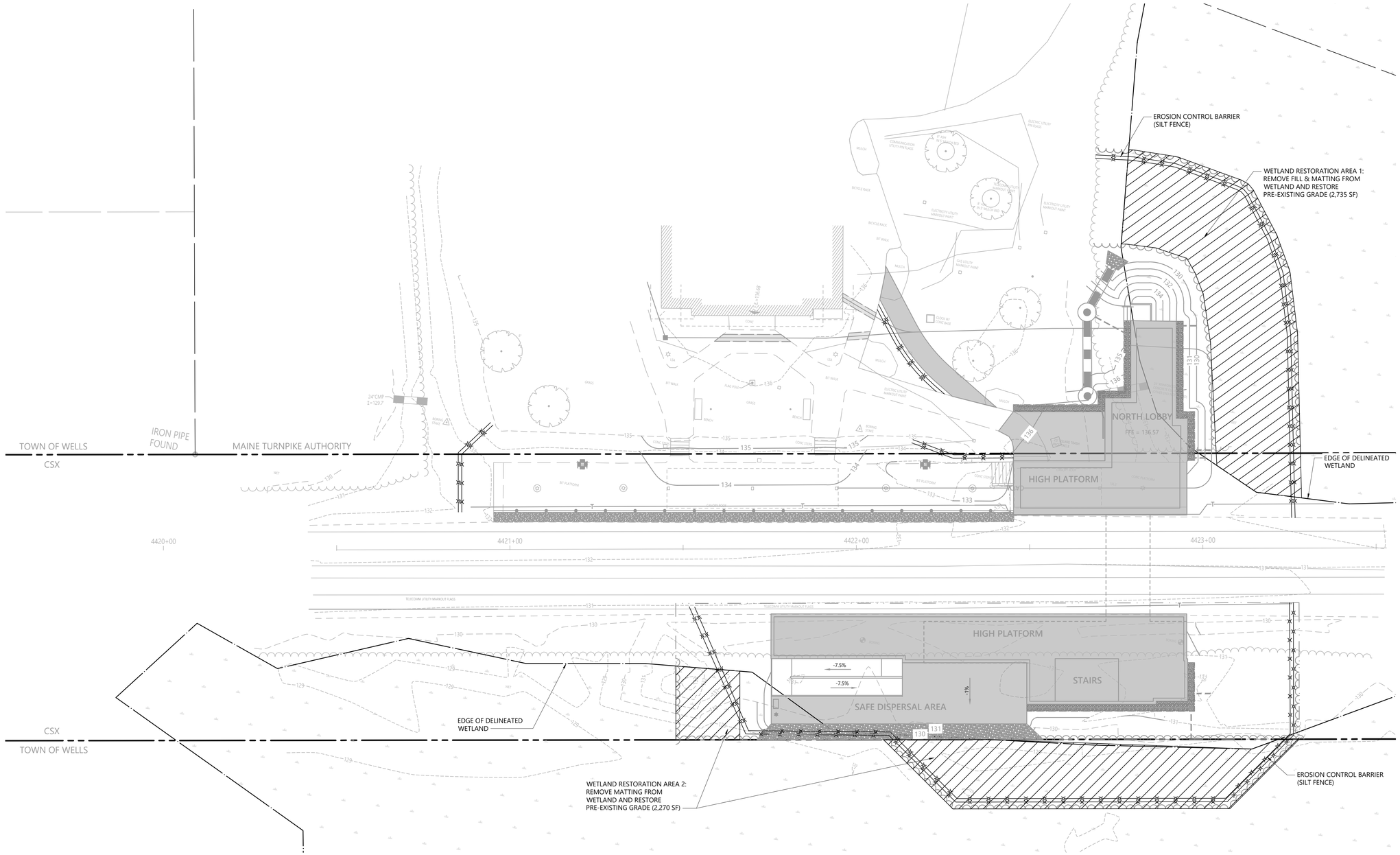
1. THE CONTRACTOR SHALL SUBMIT A WRITTEN MANUAL, PREPARED FOR THE OWNER, THAT OUTLINES A SCHEDULE FOR PROPER MAINTENANCE OF THE LAWNS. THIS SCHEDULE SHOULD INCLUDE TIMING AND METHODS FOR MOWING, WATERING, AERATION, FERTILIZATION, LIMING, AND OTHER LAWN MAINTENANCE OPERATIONS.
2. SEEDING SHALL BE DONE BETWEEN APRIL 1 TO JUNE 1, OR BETWEEN AUGUST 15 TO OCTOBER 15.
3. ALL DISTURBED AREAS NOT COVERED BY BUILDINGS, PAVING, OR OTHERWISE DEVELOPED, SHALL BE COVERED WITH 6 INCHES LOAM AND SEEDED.

SITE INSPECTION & MAINTENANCE:

1. CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS AND BEFORE AND AFTER EACH STORM EVENT.
2. CONTRACTOR SHALL MAINTAIN WRITTEN INSPECTION AND MAINTENANCE LOGS FOR THE EROSION CONTROL MEASURES FOR THE DURATION OF THE CONSTRUCTION PERIOD. LOGS SHALL BE MADE AVAILABLE TO THE OWNER, ENGINEER, MUNICIPALITY, AND MAINE DEP UPON REQUEST.
3. TEMPORARY MULCHING: ADDITIONAL MULCH SHALL BE IMMEDIATELY APPLIED TO AREAS WHERE LESS THAN 90% OF THE SOIL SURFACE IS COVERED WITH MULCH.
4. SILTSOCK BARRIERS, SILT FENCE BARRIERS, AND STONE CHECK DAMS: SILTSOCK BARRIERS, SILT FENCE, AND STONE CHECK DAMES SHALL BE REPAIRED IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THEM. SEDIMENT TRAPPED BEHIND BARRIERS/CHECK DAM SHALL BE REMOVED WHEN SEDIMENT DEPTH REACHES 6 INCHES. BARRIERS SHALL BE REPLACES WITH A TEMPORARY CHECK DAM IF THERE ARE SIGNS OF UNDERCUTTING OR IMPOUNDING LARGE VOLUMES OF WATER BEHIND THEM.
5. EROSION CONTROL BLANKETS: IF WASHOUTS OR BREAKAGE OCCURS, SLOPES SHALL BE REPAIRED, AND BLANKETS SHALL BE RE-INSTALLED.
6. STABILIZED CONSTRUCTION EXITS: EXITS SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. IF EXIT BECOMES INEFFECTIVE IT SHALL BE RECONSTRUCTED AND/OR REPLACED.
7. TEMPORARY SEDIMENTATION/DEWATERING BASINS: SEDIMENT IN TEMPORARY BASINS SHALL BE REMOVED AS NECESSARY DEPENDING ON THEIR USE AND DESIGN.
8. UPON COMPLETION OF CONSTRUCTION AND ESTABLISHMENT OF PERMANENT GROUND COVER, CONTRACTOR SHALL REMOVE AND DISPOSE OF EROSION CONTROL MEASURES AND CLEAN SEDIMENT AND DEBRIS FROM ENTIRE DRAINAGE SYSTEMS.
9. LONG-TERM MAINTENANCE OF THE PERMANENT EROSION CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE OWNER.

\\VHB\GBL\PROJ\SPORTLAND\55095.17 NNEPRA SECOND PLATFORM\CAD\LD\PLANMISC\WETLAND RESTORATION SET\55095.17WET-COV 7
December 2023

VHB Project : 55095.17
Issued for : Permitting



WETLAND RESTORATION AREA 1 SEQUENCE & NOTES:

1. INSTALL AND/OR MAINTAIN EROSION CONTROLS AT BOUNDARY OF WETLAND RESTORATION AREA. EROSION CONTROLS WILL BE MAINTAINED UNTIL COMPLETION OF THE RESTORATION OF TEMPORARILY IMPACTED WETLAND AREAS ARE COMPLETE.
2. REMOVE CLEAN STONE FILL, GRAVEL AND EROSION CONTROL GEOTEXTILE FABRIC PRIOR TO INITIATING WETLAND RESTORATION ACTIVITIES.
3. REMOVE ANY VEGETATION ESTABLISHED WITHIN TEMPORARILY FILLED AREAS, AND DISPOSE OF ANY INVASIVE SPECIES IN A SUITABLE MANNER.
4. CONDUCT GRADING WITHIN RESTORED WETLAND AREAS TO SUBSTANTIALLY MATCH PRE-EXISTING CONTOURS AND REESTABLISH MICRO TOPOGRAPHY. CONTRACTOR WILL TIE RESTORED GRADES INTO ADJACENT GRADES AS NECESSARY TO ESTABLISH HYDROLOGIC CONNECTIVITY. CONTRACTOR WILL AVOID COMPACTION OF RESTORED WETLAND SOIL SURFACES.
5. CONDUCT WETLAND PLANTINGS INCLUDING NEW ENGLAND ROADSIDE MATRIX WET MEADOW SEED MIX AND TREE PLANTINGS. SEE PROPOSED RESTORATION AREA PLANTINGS LIST AND NOTES.
6. INITIATE PROPOSED RESTORATION MONITORING PROGRAM.

WETLAND RESTORATION AREA 2 SEQUENCE & NOTES:

1. INSTALL AND/OR MAINTAIN EROSION CONTROLS AT BOUNDARY OF WETLAND RESTORATION AREA. EROSION CONTROLS WILL BE MAINTAINED UNTIL COMPLETION OF THE RESTORATION OF TEMPORARILY IMPACTED WETLAND AREAS ARE COMPLETE.
2. REMOVE CONSTRUCTION MATS AND EXISTING COIR LOGS PRIOR TO INITIATING RESTORATION PROJECT.
3. REMOVE ANY VEGETATION ESTABLISHED WITHIN TEMPORARILY FILLED AREAS, AND DISPOSE OF ANY INVASIVE SPECIES IN A SUITABLE MANNER.
4. CONDUCT GRADING WITHIN RESTORED WETLAND AREAS TO SUBSTANTIALLY MATCH PRE-EXISTING CONTOURS AND REESTABLISH MICRO TOPOGRAPHY. CONTRACTOR WILL TIE RESTORED GRADES INTO ADJACENT GRADES AS NECESSARY TO ESTABLISH HYDROLOGIC CONNECTIVITY. CONTRACTOR WILL AVOID COMPACTION OF RESTORED WETLAND SOIL SURFACES.
5. CONDUCT WETLAND PLANTINGS INCLUDING NEW ENGLAND WET MIX SEEDING AND TREE PLANTINGS. SEE PROPOSED RESTORATION AREA PLANTINGS LIST AND NOTES.
6. INITIATE PROPOSED RESTORATION MONITORING PROGRAM.

LEGEND:

WETLAND RESTORATION AREAS

NOTES:

1. RESTORATION OF TEMPORARILY IMPACTED WETLAND AREAS
   - A. IN AREAS OF AUTHORIZED TEMPORARY DISTURBANCE, IF TREES ARE CUT THEY SHALL BE CUT AT OR ABOVE GROUND LEVEL AND NOT UPROOTED IN ORDER TO PREVENT DISRUPTION TO THE WETLAND SOIL STRUCTURE AND TO ALLOW STUMP SPROUTS TO REVEGETATE THE WORK AREA, UNLESS OTHERWISE AUTHORIZED.
   - B. THE INTRODUCTION OR SPREAD OF INVASIVE PLANT SPECIES IN DISTURBED AREAS SHALL BE CONTROLLED. IF CONSTRUCTION MATS ARE TO BE USED IN AREAS OF INVASIVE PLANT SPECIES, THEY SHALL BE THOROUGHLY CLEANED BEFORE RE-USE.
   - C. WETLAND AREAS WHERE PERMANENT DISTURBANCE IS NOT AUTHORIZED SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND ELEVATION. ORIGINAL CONDITION MEANS PROTECTION AND/OR REMOVAL OF EXISTING SOIL AND VEGETATION, AND REPLACEMENT BACK TO THE ORIGINAL LOCATION SUCH THAT THE ORIGINAL SOIL LAYERING AND VEGETATION SCHEMES ARE APPROXIMATELY THE SAME, UNLESS OTHERWISE AUTHORIZED. RESTORATION SHALL TYPICALLY COMMENCE NO LATER THAN THE COMPLETION OF CONSTRUCTION.
   - D. UPON COMPLETION OF CONSTRUCTION, ALL AREAS OF AUTHORIZED DISTURBED WETLAND AREA SHALL BE STABILIZED WITH A WETLAND SEED MIX CONTAINING ONLY PLANT SPECIES NATIVE TO NEW ENGLAND AND SHALL NOT CONTAIN ANY SPECIES LISTED IN THE "INVASIVE AND OTHER UNACCEPTABLE PLANT SPECIES" APPENDIX K IN THE "NEW ENGLAND DISTRICT COMPENSATORY MITIGATION GUIDANCE" FOUND AT: WWW.NAE.USACE.ARMY.MIL/MISSIONS/REGULATORY/MITIGATION

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2 Bedford Farms Drive
Suite 200
Bedford, NH 03110
603.391.3900

0 7.5 15 30 Feet

Wells Station Expansion Restoration Plan

Wells Transportation Center
Wells, ME

No.	Revision	Date	Appvd.

Designed by DJB | Checked by TMD

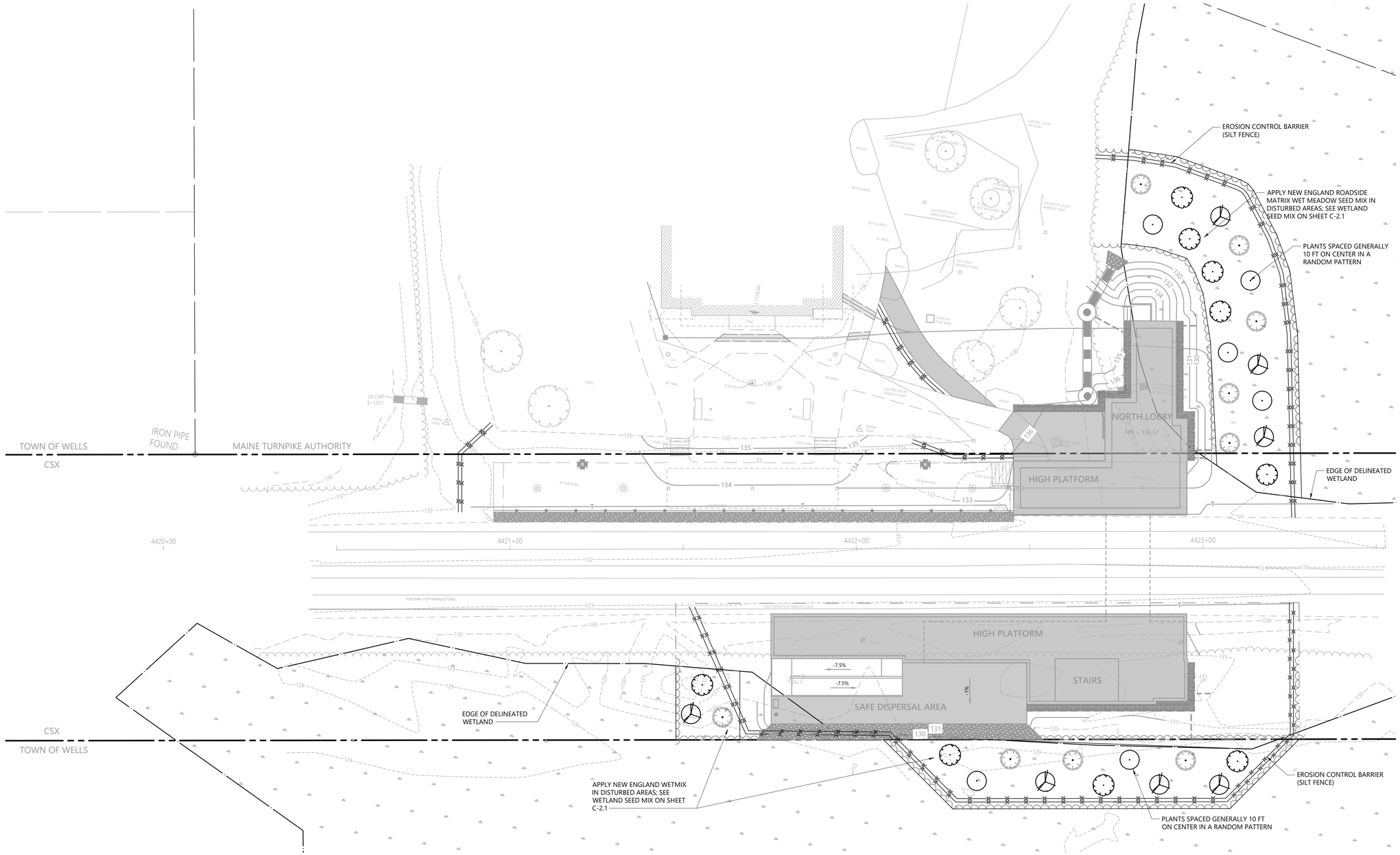
Issued for: Permitting | Date: December 7, 2023

Drawing Title: Overall Restoration Plan

Drawing Number: C-1.1

Sheet 1 of 3

Project Number: 55095.17



PLANTING NOTES:

1. NEW ENGLAND WETMIX SPECIES: FOX SEDGE (CAREX VULPINOIDEA), BLUNT BROOM SEDGE (CAREX SCORPARIA), LURID SEDGE (CAREX LURIDA), HOP SEDGE (CAREX LUPULINA), FOWL BLUEGRASS (POA PALUSTRIS), BEGGAR TICKS (BIDENS FRONDOSA), GREEN BULRUSH (SCIRPUS ATROVIRENS), SWAMP MILKWEED (ASCLEPIAS INCARNATA), FRINGED SEDGE (CAREX CRINITA), NEW YORK IRONWEED (VERNONIA NOVEBORACENSIS), SOFT RUSH (JUNCUS EFFUSUS), STARVED/CALICO ASTER (ASTER LATERIFLORUS), BLUE FLAG (IRIS VERSICOLOR), AMERICAN MANNAGRASS (GLYCERIA GRANDIS), SQUARE STEMMED MONKEY FLOWER (MIMULUS RINGENS), SPOTTED JOE PYE WEED (EUPATORIUM MACULATUM).

2. NEW ENGLAND WETMIX CAN BE SOWN BY HAND, WITH A HAND HELD SPREADER, OR HYDRO-SEEDED ON LARGE OR HARD TO REACH SITES. LIGHTLY RAKE TO INSURE GOOD SEED TO SOIL CONTACT, SEEDING CAN TAKE PLACE ON FROZEN SOIL, AS THE FREEZING AND THAWING WEATHER OF LATE FALL AND LATE WINTER WILL WORK THE SEED INTO THE SOIL. IF SPRING CONDITIONS ARE DRIER THAN USUAL WATERING MAY BE REQUIRED. IF SOWING DURING THE SUMMER MONTHS SUPPLEMENTAL WATERING WILL LIKELY BE REQUIRED UNTIL GERMINATION. A LIGHT MULCH OF CLEAN, WEED FREE STRAW IS RECOMMENDED.

3. NEW ENGLAND ROADSIDE MATRIX WET MEADOW SEED MIX SPECIES: RIVERBANK WILD RYE (ELYMUS RIPARIUS), VIRGINIA WILD RYE (ELYMUS VIRGINICUS), RED FESCUE (FESTUCA RUBRA), SWITCH GRASS (PANICUM VIRGATUM), BLUNT BROOM SEDGE (CAREX SCORPARIA), SILKY DOGWOOD (CORNUS AMOMUM), LURID SEDGE (CAREX LURIDA), BLUE FLAG (IRIS VERSICOLOR), SWAMP MILKWEED (ASCLEPIAS INCARNATA), ARROW WOOD VIBURNUM (VIBURNUM DENTATUM), NEW ENGLAND ASTER (ASTER NOVAE-ANGLIAE), BONESET (EUPATORIUM PERFOLIATUM), SPOTTED JOE PYE WEED (EUPATORIUM MACULATUM), ELDERBERRY (SAMBUCUS CANADENSIS), GREEN BULRUSH (SCIRPUS ATROVIRENS), FLAT TOPPED/UMBRELLA ASTER (ASTER UMBELLATUS).

4. NEW ENGLAND ROADSIDE MATRIX WET MEADOW SEED MIX MAY BE APPLIED BY HYDROSEEDING, OR BY MECHANICAL SPREADER. ALWAYS APPLY ON A CLEAN, WED-FREE SEED BED. AFTER SOWING, LIGHTLY RAKE OR ROLL THE SITE TO IMPROVE SEED-TO-SOIL CONTACT. BEST RESULTS ARE OBTAINED WITH A MID-LATE SPRING SEEDING. SUMMER SEEDING WILL BENEFIT FROM A LIGHT MULCHING OF CLEAN, WEED-FREE STRAW TO CONSERVE SOIL MOISTURE.

PLANT LEGEND:

RED MAPLE (ACER RUBRUM)

BALSAM FIR (ABIES BALSAMEA)

YELLOW BIRCH (BETULA ALLEGHANIENSIS)

WHITE PINE (PINUS STROBUS)

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Wells Station Expansion Restoration Plan

Wells Transportation Center
Wells, ME

No.	Revision	Date	Appvd.

Designed by DJB | Checked by TMD

Issued for: **Permitting** | Date: **December 7, 2023**

Drawing Title
Restoration Planting Plan

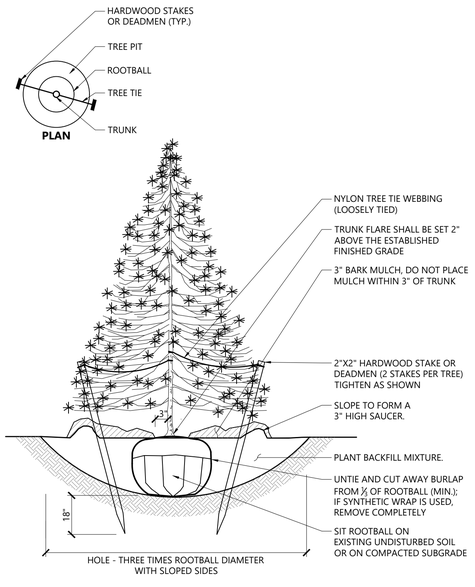
Drawing Number
C-1.2

Sheet 2 of 3

Project Number
55095.17

RESTORATION AREA PLANTINGS

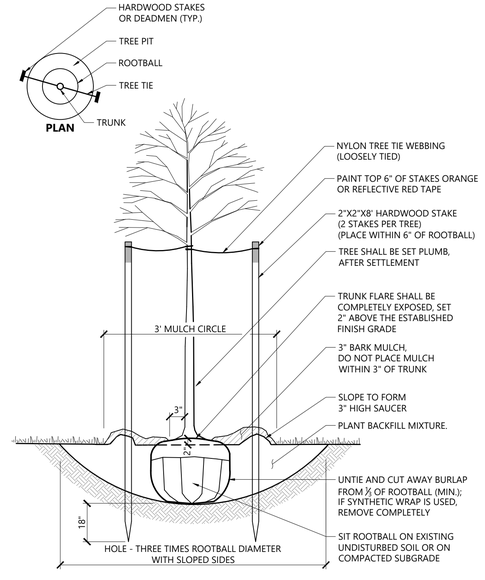
PLANT NAME	PLANT SIZE	PLANT QUANTITY	SPACING	SYMBOL
RED MAPLE (ACER RUBRUM)	4'-6' PLANTINGS	8	10 FT. OC	
BALSAM FIR (ABIES BALSAMA)	4'-6' PLANTINGS	8	10 FT. OC	
YELLOW BIRCH (BETULA ALLEGHANIENSIS)	4'-6' PLANTINGS	8	10 FT. OC	
WHITE PINE (PINUS STROBUS)	4'-6' PLANTINGS	8	10 FT. OC	
NEW ENGLAND WETMIX	SEED MIX		APPLICATION RATE: 1 LB./2500 SQ. FT.	
NEW ENGLAND ROADSIDE MATRIX WET MEADOW SEED MIX	SEED MIX		APPLICATION RATE: 1 LB./1250 SQ. FT.	



Evergreen Tree Planting

9/21

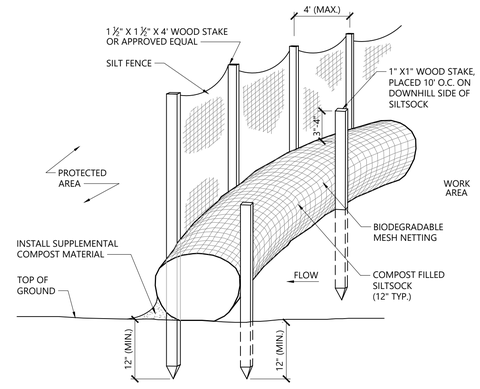
N.T.S. | Source: VHB | REV | LD_604



Tree Planting (For Trees Under 4" Caliper)

9/21

N.T.S. | Source: VHB | REV | LD_602



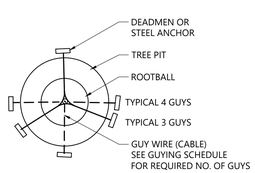
NOTES

1. SILTSOCK SHALL BE FILTREXX SILTSOXX, OR APPROVED EQUAL.
2. SILTSOCKS SHALL OVERLAP A MINIMUM OF 12 INCHES.
3. SILTSOCK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS, AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED.
4. UPON SITE STABILIZATION, COMPOST MATERIAL SHALL BE DISPERSED ON SITE, AS DETERMINED BY THE ENGINEER.
5. IF NON BIODEGRADABLE NETTING IS USED THE NETTING SHALL BE COLLECTED AND DISPOSED OF OFFSITE.

Siltsock / Silt Fence Barrier

10/20

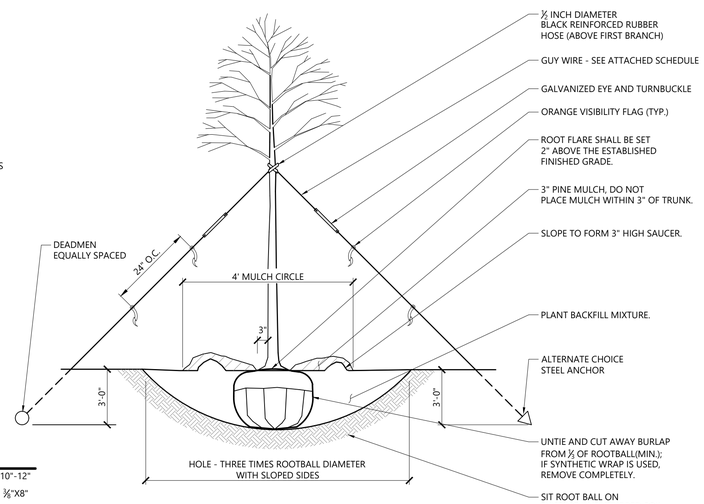
N.T.S. | Source: VHB | REV | LD_658-A



PLAN

NOTES

1. USE GUYING SCHEDULE BELOW FOR DECIDUOUS AND EVERGREEN TREES OVER 4" CALIPER.



GUYING SCHEDULE

TREE CALIPER	4"-6"	6"-8"	8"-10"	10"-12"
TURNBUCKLE (GALVANIZED	⅜"X 4-½"	⅜"X 4-½"	⅜"X6"	⅜"X8"
# GUYS REQ'D	3	3	4	4
WIRE OR CABLE	⅛"DIA.	3/16"DIA.	¼"DIA.	5/16"DIA.
DEADMEN SIZE	4"DIA.	6"DIA.	8"DIA.	10"DIA.
DEADMEN LENGTH	24"	24"	36"	48"

Tree Planting (For Trees Over 4" Caliper)

1/16

N.T.S. | Source: VHB | REV | LD_603

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Wells Station Expansion Restoration Plan

Wells Transportation Center
Wells, ME

No.	Revision	Date	Appvd.

Designed by: DJB | Checked by: TMD

Issued for: Permitting | Date: December 7, 2023

Drawing Title: Site Details

Drawing Number: C-2.1

Sheet 3 of 3

Project Number: 55095.17

\\vhb\gbl\proj\SPortland\55095.17 NNEPRA Second Platform\cad\sr\planset\55095.17 EX_ECP.dwg

N

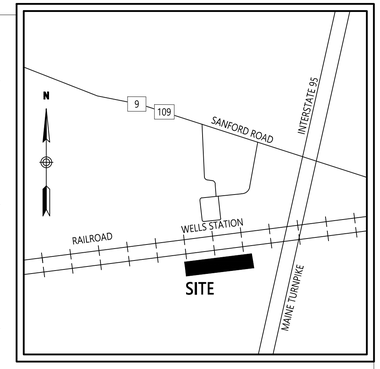
GRID NORTH - NAD 83
MAINE WEST ZONE (2-ZONE SYSTEM)

10 0 10 20
SCALE IN FEET

NOW OR FORMERLY
MAINE TURNPIKE AUTHORITY
BOOK 7745 PAGE 306
BOOK 4271 PAGE 106
BOOK 4271 PAGE 104
BOOK 2555 PAGE 288
MAP 50 LOT 19EXE

NOW OR FORMERLY
CSX CORPORATION

NOW OR FORMERLY
TOWN OF WELLS
BOOK 1230 PAGE 339
MAP 42 LOT 8EXE



SITE LOCUS
NOT TO SCALE

Legend

- DRAIN MANHOLE
- CATCH BASIN
- (S) SEWER MANHOLE
- (E) ELECTRIC MANHOLE
- (T) TELEPHONE MANHOLE
- MANHOLE
- HH HAND HOLE
- WATER GATE
- FIRE HYDRANT
- GAS GATE
- BOLLARD w/LIGHT
- STREET SIGN
- LIGHT POLE
- UTILITY POLE
- GUY POLE
- GUY WIRE
- MONITORING WELL
- FLOOD LIGHT
- (W) WELL
- MARSH
- F.F.E.=45.27' FINISHED FLOOR ELEVATION
- CNO COULD NOT OPEN
- NPV NO PIPES VISIBLE
- DYL DOUBLE YELLOW LINE
- DWL DASHED WHITE LINE
- SYL SINGLE YELLOW LINE
- LSA LANDSCAPED AREA
- EOP EDGE OF PAVEMENT
- CC CONCRETE CURB
- VGC VERTICAL GRANITE CURB
- SGE SLOPED GRANITE EDGE
- BB BITUMINOUS BERM
- BC BITUMINOUS CURB
- GUARD RAIL
- CHAIN LINK FENCE
- DRAINAGE LINE
- SEWER LINE
- OHW OVERHEAD WIRE
- E UNDERGROUND ELECTRIC
- T TELEPHONE LINE
- G GAS LINE
- W WATER LINE
- STONE WALL
- TREE LINE
- 100'BZ 100-FT BUFFER ZONE
- 100'RA 100-FT RIVER FRONT AREA
- 200'RA 200-FT RIVER FRONT AREA
- AFI-100 LIMIT MEAN ANNUAL HIGH WATER
- BFI-100 LIMIT OF BANK
- WFI-100 VEGETATED WETLAND BOUNDARY

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Wells Transportation Center

Off Sanford Road
Wells, Maine

No.	Revision	Date	Appvd.

Designed by | Checked by

Issued for | Date

October 13, 2022

Not Issued for Construction

Drawing Title

Existing Conditions Plan

Drawing Number

EC-1

Sheet 1 of 1

Project Number
55095.17

US Army Corps
of Engineers ®
New England District

(Minimum Notice: Permittee must sign and return notification within one month of the completion of work.)

COMPLIANCE CERTIFICATION FORM

Corps of Engineers Permit No: NAE-2023-00888

Name of Permittee: Jim Russell, NNEPRA

Permit Issuance Date: February 27, 2024

Please sign this certification and return it to the following address upon completion of the activity and any mitigation required by the permit. You must submit this after the mitigation is complete, but not the mitigation monitoring, which requires separate submittals.

****
* MAIL TO: U.S. Army Corps of Engineers, New England District *
* Policy & Technical Support Branch *
* Regulatory Division *
* 696 Virginia Road *
* Concord, Massachusetts 01742-2751 *
****

Please note that your permitted activity is subject to a compliance inspection by an U.S. Army Corps of Engineers representative. If you fail to comply with this permit you are subject to permit suspension, modification, or revocation.

I hereby certify that the work authorized by the above referenced permit was completed in accordance with the terms and conditions of the above referenced permit, and any required mitigation was completed in accordance with the permit conditions.

_____ _____
Signature of Permittee Date

_____ _____
Printed Name Date of Work Completion

(_____)_____ (_____)_____
Telephone Number Telephone Number

US Army Corps
of Engineers ®
New England District

GENERAL PERMIT
WORK-START NOTIFICATION FORM
(Minimum Notice: Two weeks before work begins)

EMAIL TO: natalie.bingham@usace.army.mil

-or-

MAIL TO: Natalie Bingham
U.S. Army Corps of Engineers, New England District
Maine Project Office
442 Civic Center Drive, Suite 350
Augusta, Maine 04330

A Corps of Engineers Permit (No. NAE-2023-00888) was issued to Jim Russell, NNEPRA. The permit authorized the permittee to place approximately 1,463 SF of permanent and 4,997 SF of temporary fill in freshwater wetlands off 696 Sanford Road at Wells, Maine in order to construct a new side platform and access to freight main line No. 2 Track.

The people (e.g., contractor) listed below will do the work, and they understand the permit's conditions and limitations.

PLEASE PRINT OR TYPE

Name of Person/Firm: _____

_____

_____

Business Address: _____

_____

_____

Telephone: (____) _____ (____) _____

Proposed Work Dates: Start: _____

Finish: _____

PERMITTEE'S SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

FOR USE BY THE CORPS OF ENGINEERS

Project Manager: BINGHAM Submittals Required: Work-Start and Compliance forms

Inspection Recommendation: routine Maine General Permits compliance

Maine Department of Environmental Protection Permit

STATE OF MAINE
DEPARTMENT OF ENVIRONMENTAL PROTECTION
Permit-by-Rule & Notice of Intent Review Form

Natural Resources Protection Act — **PBR #** 80038
Stormwater Management Law — **PBR #**
Maine Construction General Permit — **NOI #**

Applicant: Maine Turnpike Authority — **Town:** Wells
Project Address: 696 Sandford Road — **Tax Map/Lot #:** 50-19

NRPA PBR Sections – Ch. 305

- ☐ Sec. 2 Act. Adj. to Prot. Natural Res.
- ☐ Sec. 3 Intake Pipes
- ☐ Sec. 4 Replacement of Structures
- ☐ Sec. 6 Movement of Rocks or Veg.
- ☐ Sec. 7 Outfall Pipes
- ☐ Sec. 8 Shoreline Stabilization
- ☐ Sec. 9 Utility Crossing
- ☐ Sec. 10 Stream Crossing
- ☑ Sec. 11 State Transport. Facilities
- ☐ Sec. 12 Restoration Natural Areas
- ☐ Sec. 13 F&W Creat./Water Quality
- ☐ Sec. 15 Public Boat Ramps
- ☐ Sec. 16 Coastal Sand Dune Project
- ☐ Sec. 17 Transfer/Permit Extension
- ☐ Sec. 18 Maintenance Dredging
- ☐ Sec. 19 Act. Near SVP Habitat
- ☐ Sec. 20 Act. Near Waterfowl/Bird Habitat

Notes:
Your Permit-By-Rule Notification Form was received and reviewed by the DEP and was found to be acceptable. Please acknowledge the following standards prior to beginning work.

C. Standards
(1) The following measures must be taken to prevent erosion of soil or fill material from disturbed areas into the resource:
(a) Staked hay bales or silt fence must be properly installed between the area of soil disturbance and the resource before the activity begins;
(b) Hay bales or silt fence barriers must be maintained until the disturbed area is permanently stabilized;
(c) Within 7 calendar days following the completion of any soil disturbance, and prior to any storm event, mulch must be spread on any exposed soils;
(d) All disturbed soils must be permanently stabilized; and
(e) Within 30 days of final stabilization of the site, any silt fence must be removed.

Photographs showing the completed project and the affected area must be submitted within 20 days of the activity's completion. The photographs must be sent with a copy of the notification form or labeled with the applicant's name and the town in which the activity took place.

Project accepted

Reviewer: — **Deficient Date:** ☐ NRPA ☐ SW ☐ NOI
Reviewer: Sierra Swett — **Accepted Date:** 07/15/2021 ☑ NRPA ☐ SW ☐ NOI

11. State transportation facilities

A. Applicability

(1) This section applies to the maintenance, repair, reconstruction, rehabilitation, replacement or minor construction of a State Transportation Facility carried out by, or under the authority of, the Maine Department of Transportation (MaineDOT) or the Maine Turnpike Authority, including any testing or preconstruction engineering, and associated technical support services.

(2) This section does not apply to an activity within a coastal sand dune system.

NOTE: The construction of a transportation facility other than roads and associated facilities may be subject to the Storm Water Management Law, 38 M.R.S.A. Section 420-D.

B. Standards

(1) Photographs of the area to be altered by the activity must be taken before work on the site begins. The photographs must be kept on file and be made available at the request of the DEP.

(2) The activity must be reviewed by the Department of Inland Fisheries and Wildlife and the Department of Marine Resources, as applicable. The applicant must coordinate with the reviewing agencies and incorporate any recommendations from those agencies into the performance of the activity.

(3) All construction activities undertaken must be detailed in a site-specific Soil Erosion and Water Pollution Control Plan and conducted in accordance with MaineDOT's Best Management Practices for Erosion and Sediment Control, dated January 2000, and Standard Specifications, dated December 2002.

(4) Alignment changes may not exceed a distance of 200 feet between the old and new center lines in any natural resource.

(5) The activity may not alter more than 300 feet of shoreline (both shores added together) within a mile stretch of any river, stream or brook, including any bridge width or length of culvert.

(6) The activity may not alter more than 150 feet of shoreline (both shores added together) within a mile stretch of any outstanding river segment identified in 38 M.R.S.A. 480-P, including any bridge width or length of culvert.

(7) The activity must minimize wetland intrusion. The activity is exempt from the provisions of Chapter 310, the Wetland and Waterbodies Protection Rules, if the activity alters less than 15,000 square feet of natural resources per mile of roadway (centerline measurement) provided that the following impacts are not exceeded within the 15,000 square foot area:

(a) 1,000 square feet of coastal wetland consisting of salt tolerant vegetation or shellfish habitat; or

(b) 5,000 square feet of coastal wetland not containing salt tolerant vegetation or shellfish habitat; or

(c) 1,000 square feet of a great pond.

All other activities must be performed in compliance with all sections of Chapter 310, the Wetland Protection Rules, except 310.2(C), 5(A), 9(A), 9(B) and 9(C).

(8) The activity may not permanently block any fish passage in any watercourse containing fish. The applicant must coordinate with the reviewing agencies listed in paragraph 2 above to improve fish passage and incorporate any recommendations from those agencies into the performance of the activity.

NOTE: For guidance on meeting the design objectives for fish passage, including peak flow, maximum velocity, mining depth and gradient, see the MaineDOT Waterbody and Wildlife Crossing Policy and Design Guide (July 2008), developed in conjunction with state and federal resource and regulatory agencies.

(9) Rocks may not be removed from below the normal high water line of any coastal wetland, freshwater wetland, great pond, river, stream or brook except to the minimum extent necessary for completion of work within the limits of construction.

(10) If work is performed in a river, stream or brook that is less than three feet deep at the time and location of the activity, the applicant must isolate the work area from the resource and divert stream flows around the work area, maintaining downstream flows while work is in progress.

(11) Wheeled or tracked equipment may not operate in the water. Equipment operating on the shore may reach into the water with a bucket or similar extension. Equipment may cross streams on rock, gravel or ledge bottom. If avoiding the operation of wheeled or tracked equipment in the water is not possible, the applicant must explain the need to operate in the water. Approval from the DEP to operate in the water must be in writing, and any recommendations from the DEP must be incorporated into the performance of the activity.

(12) All wheeled or tracked equipment that must travel or work in a vegetated wetland area must travel and work on mats or platforms.

(13) Any debris or excavated material must be stockpiled either outside the wetland or on mats or platforms. Erosion and sediment control best management practices must be used, where necessary, to prevent sedimentation. Any debris generated during the activity must be prevented from washing downstream and must be removed from the wetland or water body. Disposal of debris must be in conformance with the Maine Hazardous Waste, Septage and Solid Waste Management Act, 38 M.R.S.A. Section 1301 *et seq.*

(14) Work below the normal high water line of a great pond, river, stream or brook must be done at low water except for emergency work or work agreed to by the resource agencies listed in paragraph 2 above.

(15) Perimeter controls must be installed before the work starts. Disturbance of natural resources beyond the construction limits shown on the plans is not allowed under this rule.

NOTE: Guidance on the location of construction limits can be obtained from the on site Construction Manager.

(16) The use of untreated lumber is preferred. Lumber pressure treated with chromated copper arsenate (CCA) may be used only if necessary and only if use is allowed under federal law and not prohibited from sale under 38 M.R.S.A. 1682, and provided it is cured on dry land in a manner that exposes all surfaces to the air for a period of at least 21 days prior to construction. Wood treated with creosote or pentachlorophenol may not be used where it will contact water.

(17) A temporary road for equipment access must be constructed of crushed stone, blasted ledge, or similar materials that will not cause sedimentation or restrict fish passage. Such roads must be completely removed at the completion of the activity. In addition, any such temporary roads which are in rivers, streams or brooks, must allow for a passage of stormwater flows associated with a 10-year storm.

(18) Non-native species may not be planted in restored areas.

(19) Disposal of debris must be in conformance with Maine Hazardous Waste, Septage and Solid Waste Management Act, 38 M.R.S.A. Sections 1301 *et seq*.

(20) Disturbance of vegetation must be avoided, if possible. Where vegetation is disturbed outside of the area covered by any road or structure construction, it must be reestablished immediately upon completion of the activity and must be maintained.

(21) A vegetated area at least 25 feet wide must be established and maintained between any new stormwater outfall structure and the high water line of any open water body. A velocity reducing structure must be constructed at the outlet of the stormwater outfall that will create sheet flow of stormwater, and prevent erosion of soil within the vegetated buffer. If the 25 foot vegetated buffer is not practicable, the applicant must explain the reason for a lesser setback in writing. Approval from the DEP must be in writing and any recommendations must be incorporated into the activity.

C. Definitions. The following terms, as used in this chapter, have the following meanings, unless the context indicates otherwise:

(1) **Diversion**. The rerouting of a river, stream or brook around a construction site and then back to the downstream channel.

(2) **Fill**. a. (verb) To put into or upon, supply to, or allow to enter a water body or wetland any earth, rock, gravel, sand, silt, clay, peat, or debris; b. (noun) Material, other than structures, placed in or immediately adjacent to a wetland or water body.

(3) **Floodplain wetlands**. Freshwater wetlands that are inundated with flood water during a 100-year flood event based on flood insurance maps produced by the Federal Emergency Agency or other site specific information.

(4) **Riprap**. Heavy, irregularly shaped rocks that are fit into place, without mortar, on a slope as defined in the MaineDOT Standard Specifications, dated December 2002.

United States Department of the Interior
Fish and Wildlife Service Requirements

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Maine Ecological Services Field Office
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East Orland, ME 04431
Phone: (207) 469-7300 Fax: (207) 902-1588

In Reply Refer To: July 18, 2023
Project code: 2023-0105798
Project Name: Well Area Improvements Project - Wells Station

Subject: Consistency letter for the 'Well Area Improvements Project - Wells Station' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated July 18, 2023 to verify that the **Well Area Improvements Project - Wells Station** (Proposed Action) may rely on the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action will have no effect on the endangered Indiana bat (*Myotis sodalis*) or the endangered northern long-eared bat (*Myotis septentrionalis*). If the Proposed Action is not modified, **no consultation is required for these two species.** If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

Well Area Improvements Project - Wells Station

DESCRIPTION

The project consists of the construction of a new passenger platform at the Wells Station. This project does include tree-cutting and freshwater wetland impact.

The approximate location of the project can be viewed in Google Maps: https://www.google.com/maps/@43.320529699999994,-70.61207018265537,14z

DETERMINATION KEY RESULT

Based on the information you provided, you have determined that the Proposed Action will have no effect on the endangered Indiana bat and/or the endangered northern long-eared bat. Therefore, no consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required for these two species.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat[1]?

   [1] See Indiana bat species profile

   Automatically answered
   No

2. Is the project within the range of the northern long-eared bat[1]?

   [1] See northern long-eared bat species profile

   Automatically answered
   Yes

3. [Semantic] Does your proposed action intersect an area where Indiana bats and northern long-eared bats are not likely to occur?

   Automatically answered
   Yes

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on June 14, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects. The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

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LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Railroad Administration